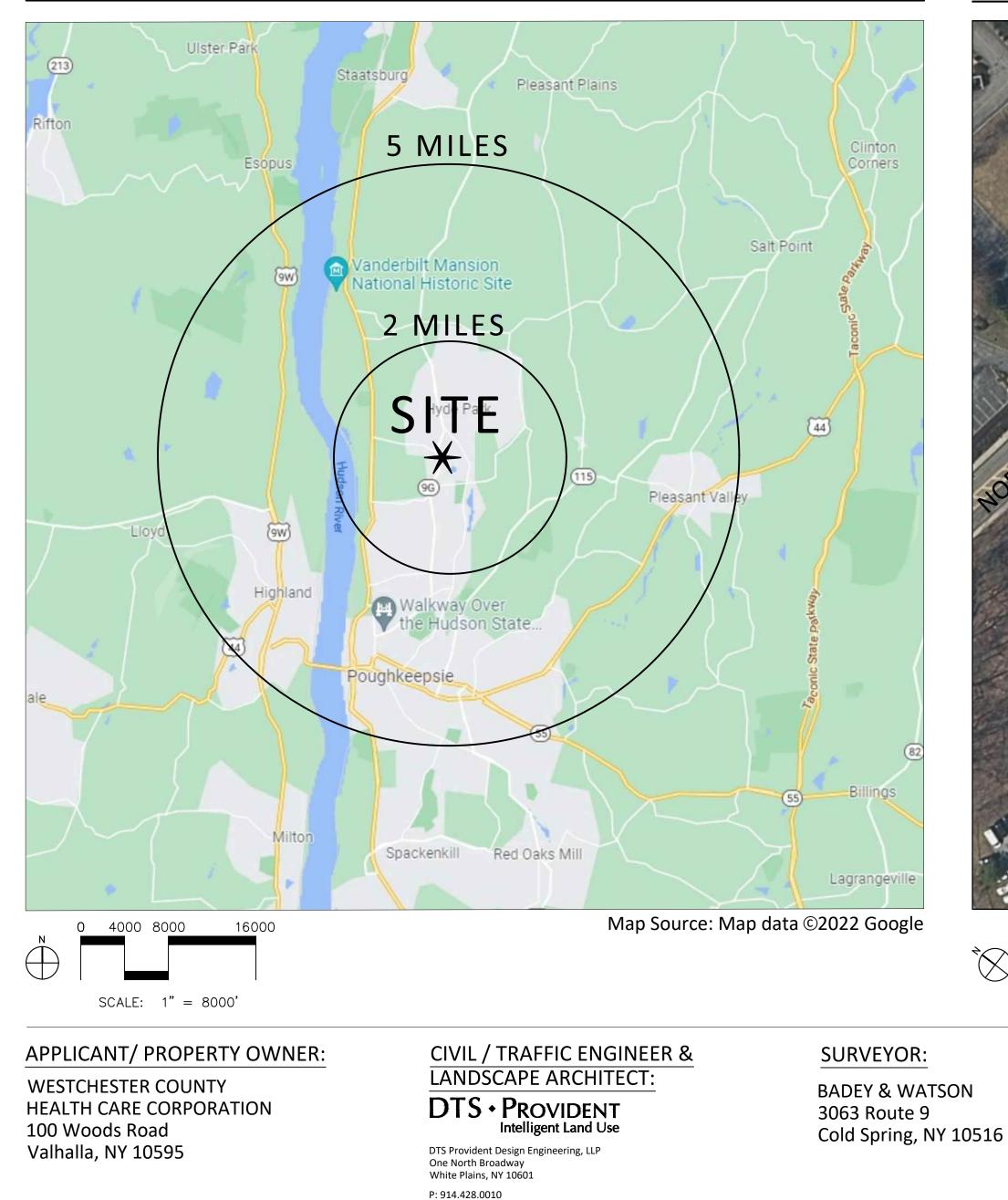
	PSIE DEPARTMENTAL SIGNATURES fore Chairman's Signature)	
Planning Department	Date	
Building Department	Date	
Fire Department	Date	
Water Department	Date	
Sewer Department	Date	
Zoning Administrator	Date	
Engineering Department	Date	

MID-HUDSON REGIONAL HOSPITAL Poughkeepsie, New York PAVEMENT REPAIRS AT BAKER AVENUE AND HELIPAD CONTRACT DOCUMENT SUBMISSION WMC CONTRACT NUMBER WMC-3829 OGS PROJECT NO. SE391 PERMIT SET - 09/06/23

SITE LOCATION MAP



F: 914.428.0017 CA#0017846



WORK AREA AERIAL

Νo. C-101 C-102 C-201 C-202 C-301 C-302 C-401 C-402 C-501 C-502 C-601 C-602 C-701 ORTHR C-801 CT-101 E-001 E-002 E-003 E-101 E-102 E-103 E-701 608-1 609.01 619-010 619-011 619-322 685-01 160

TOTAL SITE DISTURBANCE: 1.54 ACRES SOIL DISTURBANCE: 0.77 ACRES

SCALE: 1" = 80'

OWN OF POUGHKEEPSIE PLANNING BOARE PLAN APPROVAL

ne plan of development for the property as depicted hereon was approved by a majority of the members of the Poughkeepsie Planning Board at a meeting held on Approval have been satisfied or arrangements have been made to ensure the completion o any outstanding or incomplete condition

Chairmar

LIST OF DRAWINGS

	Title	Date	Scale	•
	Cover Sheet			
	Survey	03/03/22	1"=20'	
	, Layout Plan	09/06/23	1"=20'	
	, Layout Plan	09/06/23	1"=20'	
	, Construction Plan	09/06/23	1"=20'	
	Construction Plan	09/06/23	1"=20'	
	Removals Plan	09/06/23	1"=20'	
	Removals Plan	09/06/23	1"=20'	
	Marking and Signage Plan	09/06/23	1"=20'	
	Marking and Signage Plan	09/06/23	1"=20'	
	Erosion Control Plan	09/06/23	1"=20'	
	Erosion Control Plan	09/06/23	1"=20'	
	Details	09/06/23	N/A	
	Details	09/06/23	N/A	
	General Notes and Tables	09/06/23	N/A	
	Temporary Helipad Site Layout Plan	09/06/23	1 " = 2 0 '	
	Traffic Control Plan	09/06/23	1"=50'	
	Electrical Symbols, Abv. and General Notes	09/06/23	N/A	
	Electrical Specs.	09/06/23	N/A	
	Electrical Specs Cont.	09/06/23	N/A	
	Electrical Site Plan - Demolition	09/06/23	1"=30'	
	Electrical Site Plan	09/06/23	1"=30'	
	Electrical Site Plan - Light Photometrics	09/06/23	1"=30'	
	Electrical Details	09/06/23	N/A	
	Pedestrian Facilities, Sheets 1-12	08/18/21	N/A	
	Concrete and Stone Curb and Gutter, Sheets 1-4	08/04/21	N/A	
	Work Zone Traffic Control General Notes, Sheets 1-2	12/21/22	N/A	
	Work Zone Traffic Control General Tables and Legend, Sheets 1-2	12/21/22	N/A	
	Work Zone Traffic Control Two-Lane Two Way roadway Crosswalk Closure and Pedestrian Detour Short Term Operations, Sheets 1-2	04/08/22	N/A	
	Pavement Marking Details, Sheets 1-9	09/27/22	N/A	
E SHI	EET C-701 FOR NYSDOT STANDARD SHEET NUMBER AND TITLE			

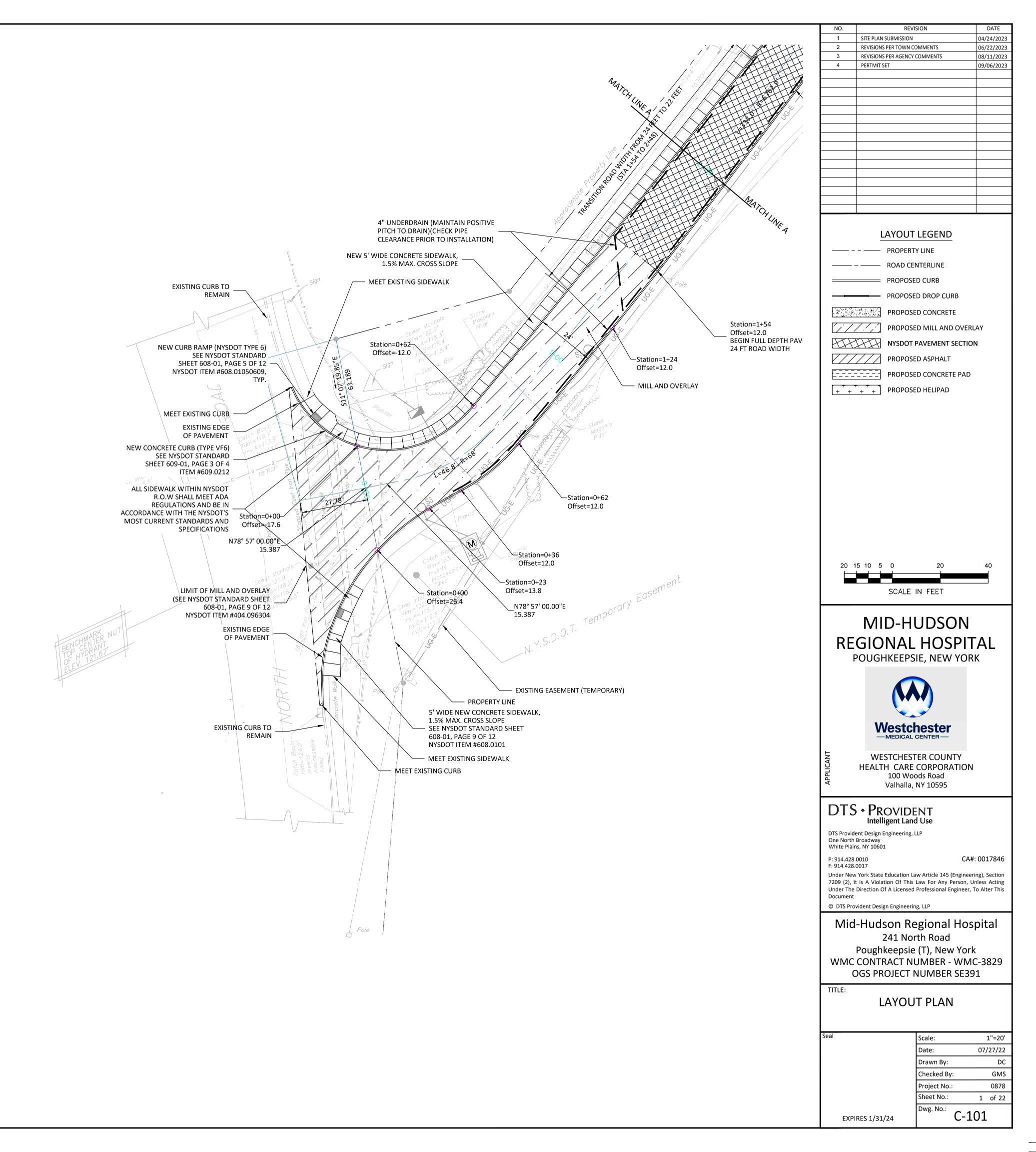
NOTE: SEE SHEET C-701 FOR NYSDOT STANDARD SHEET NUMBER AND TITLE

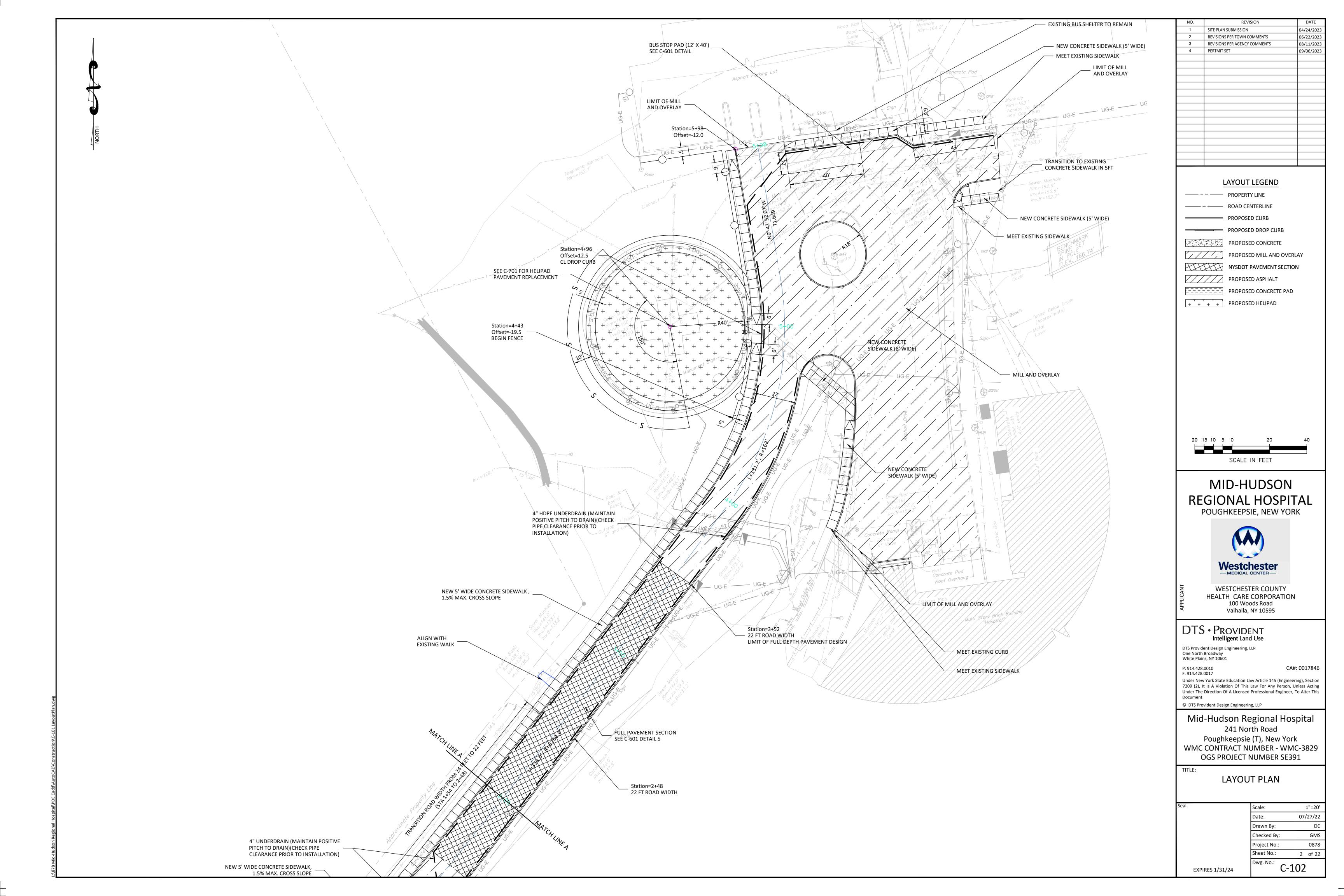
UNIFORM CODE STATEMENT

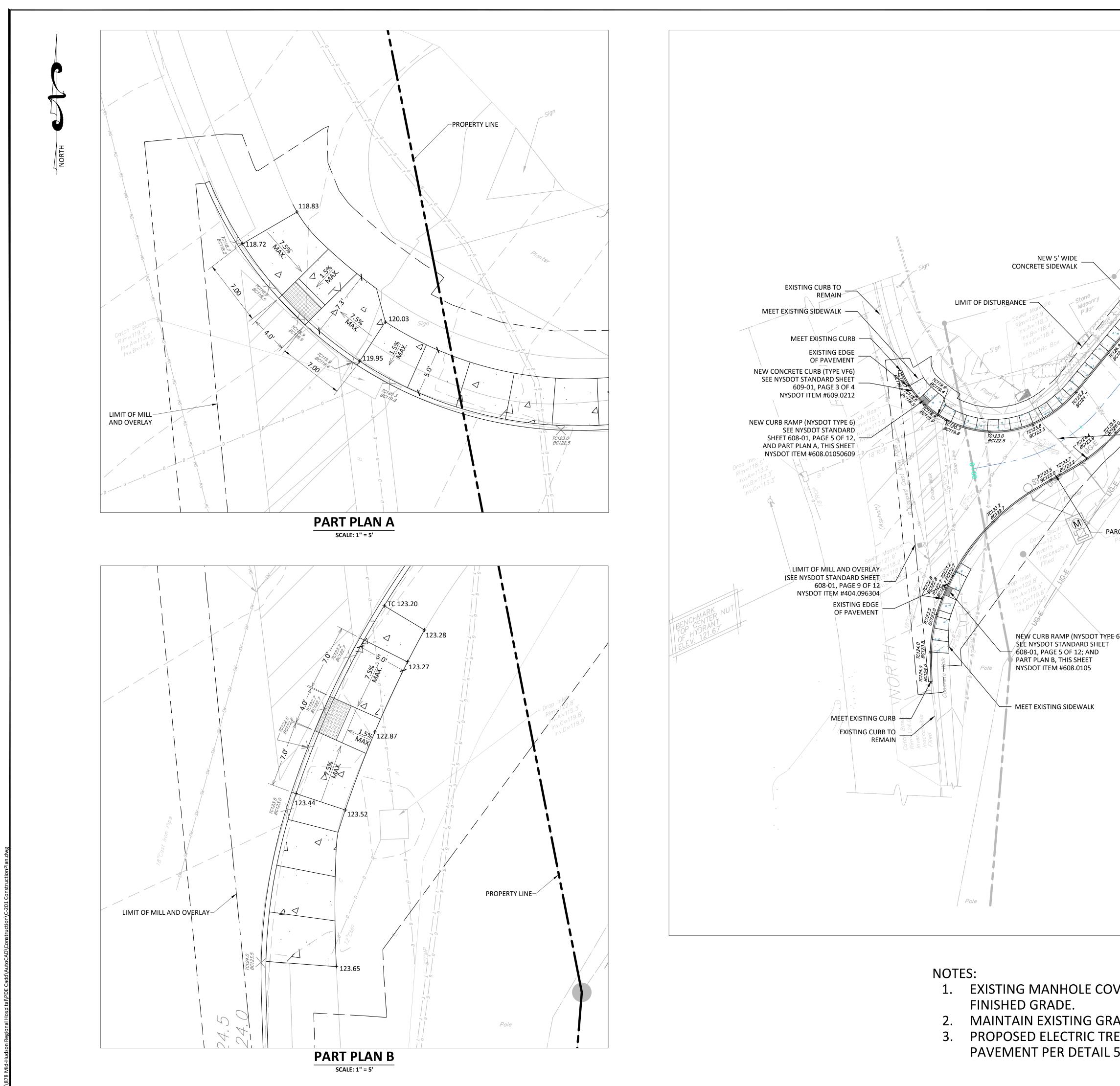
TO THE BEST OF THE REGISTERED DESIGN PROFESSIONAL'S KNOWLEDGE, BELIEF AND PROFESSIONAL JUDGEMENT, THESE PLANS AND/OR SPECIFICATIONS ARE IN COMPLIANCE WITH THE 2020 UNIFORM CODE.

Mid-Hudson Regional Hospital/PDE Cadd/AutoCAD\Construction\C-101 LayoutPlan.dwg

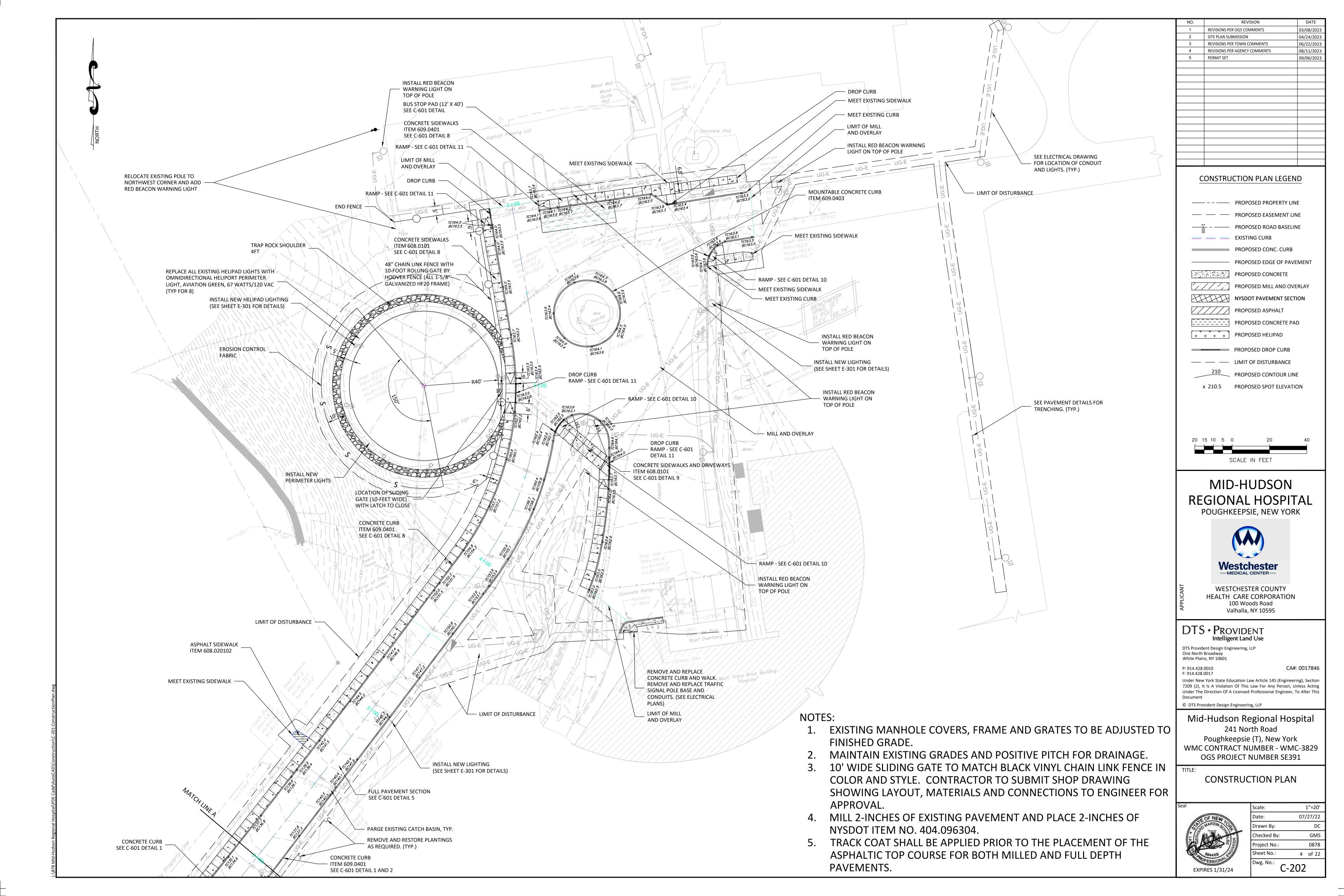






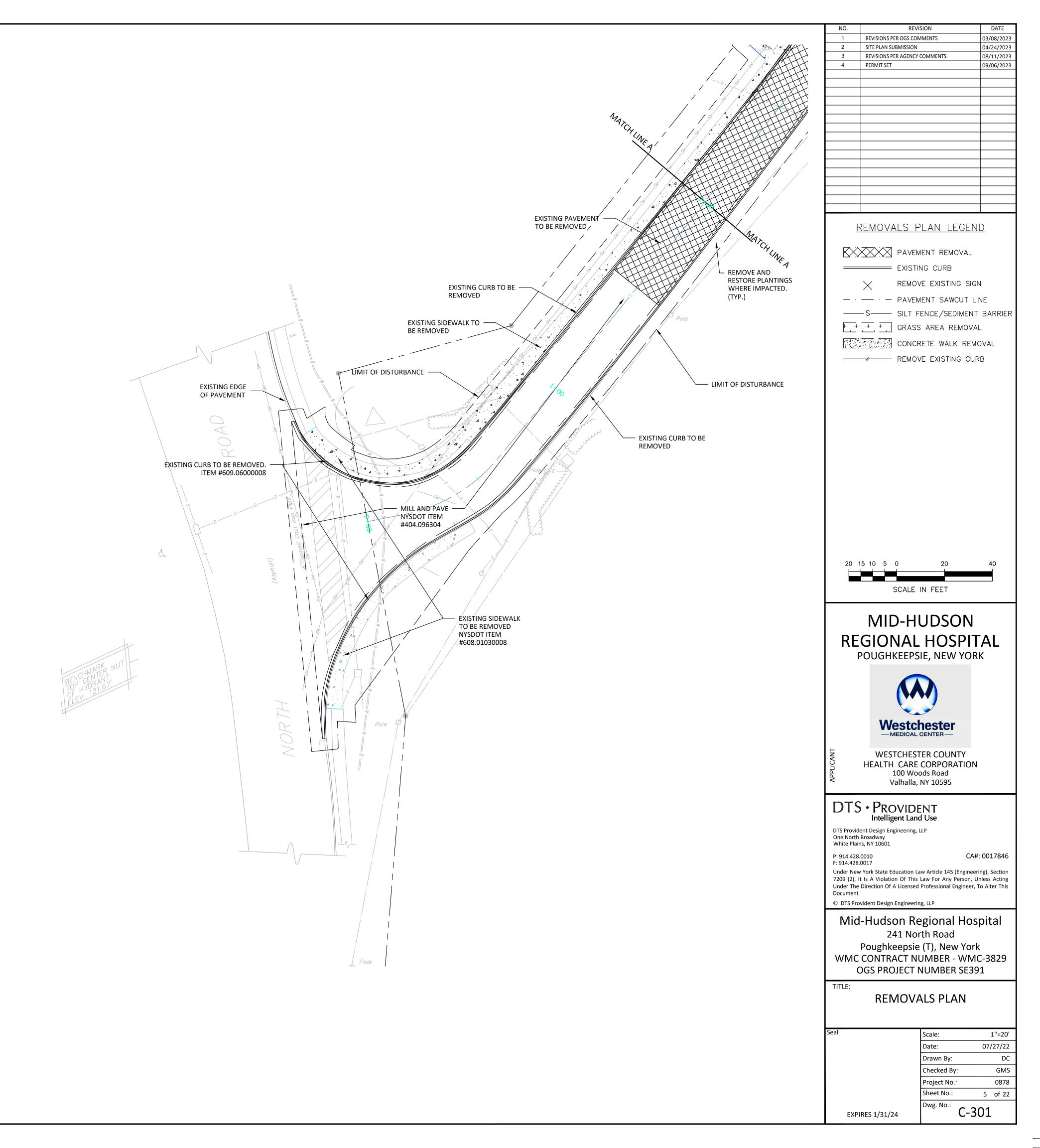


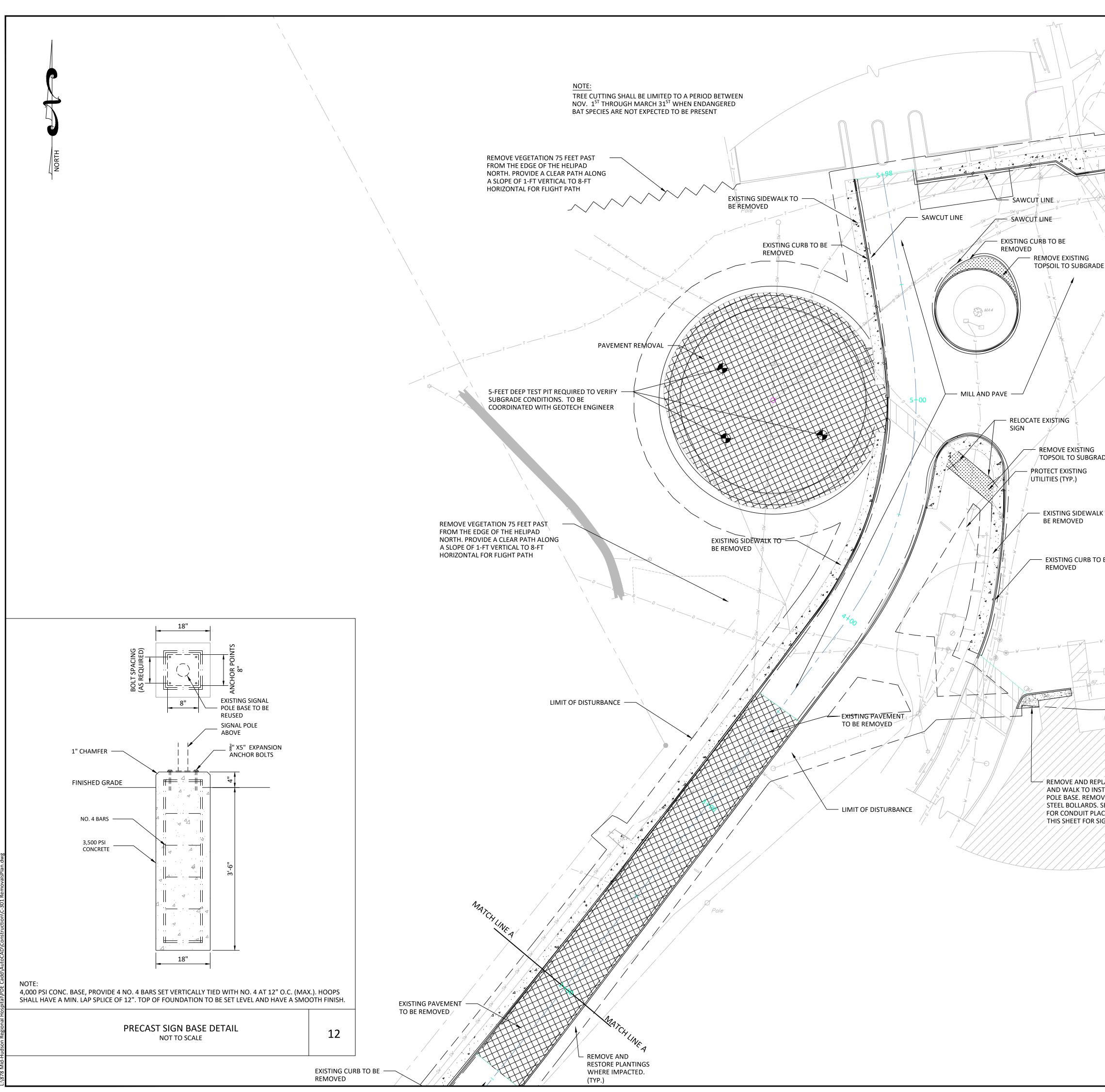
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	4	REVISIONS PER TOWN		06/22/2023 08/11/2023
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			POSED CONCRETE	
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(SEE SHEET E-301 FOR DETAILS)	+	<u>+ + +</u> PRO	POSED HELIPAD	
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		Intelligent La	nd Use	
	One Nort	dent Design Engineerin h Broadway ins, NY 10601	g, LLP	
	P: 914.42	8.0010	C	CA#: 0017846
		w York State Education		
		It Is A Violation Of The Direction Of A License t		
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			orth Road	ark
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	TITLE:	CONCTOUR		
		CONSTRU	CTION PLA	NN
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	Seal		Scale:	1"=20'
ADES AND POSITIVE PITCH FOR DRAINAGE.			Date: Drawn By:	07/27/22 DC
ENCH PAVEMENT SHALL BE FULL DEPTH			Checked By:	GMS
5 ON DRAWING C-601			Project No.: Sheet No.:	0878 3 of 22
			Dwg. No.:	
	EXI	PIRES 1/31/24	C-	·201



Mid-Hudson Regional Hospital/PDE Cadd/AutoCAD/Construction/C-301 RemovalsPlan.dwg







Mid-Hudson Regional Hospital/PDE Cadd/AutoCAD/Construction/C-301 Remo

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IGNAL POLE BASE.	

NO		DEVICIÓN	
NO. 1	REVISIONS	REVISION ER OGS COMMENTS	DATE 03/08/2023
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3	REVISIONS PE	ER AGENCY COMMENTS	08/11/2023
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NOTE:

1. ALL PAVEMENT MARKINGS SHALL BE 0.51mm (20 mils) THICK EPOXY PAINT REFLECTORIZED PAVEMENT MARKINGS WITH REFLECTIVE GLASS BEADS (20LBS/GALLON OF EPOXY RESIN) MEETING NYSDOT STANDARD SPECIFICATIONS 685 AND 727-05

ROAD

SEE NYSDOT STANDARD SHEET 685-01, PAGE 2 OF 9 -

NYSDOT ITEM #685.11

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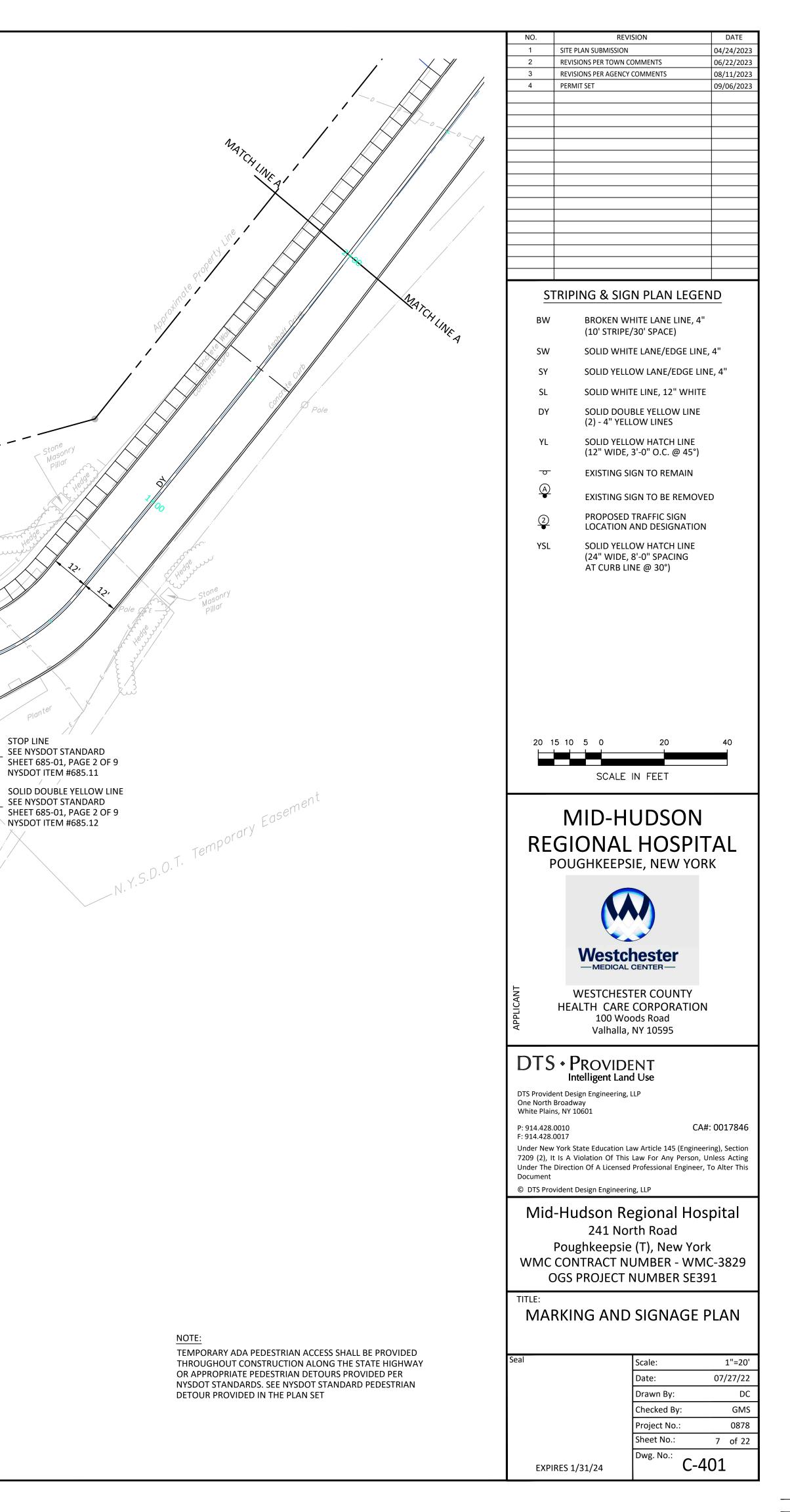
STOP LINE

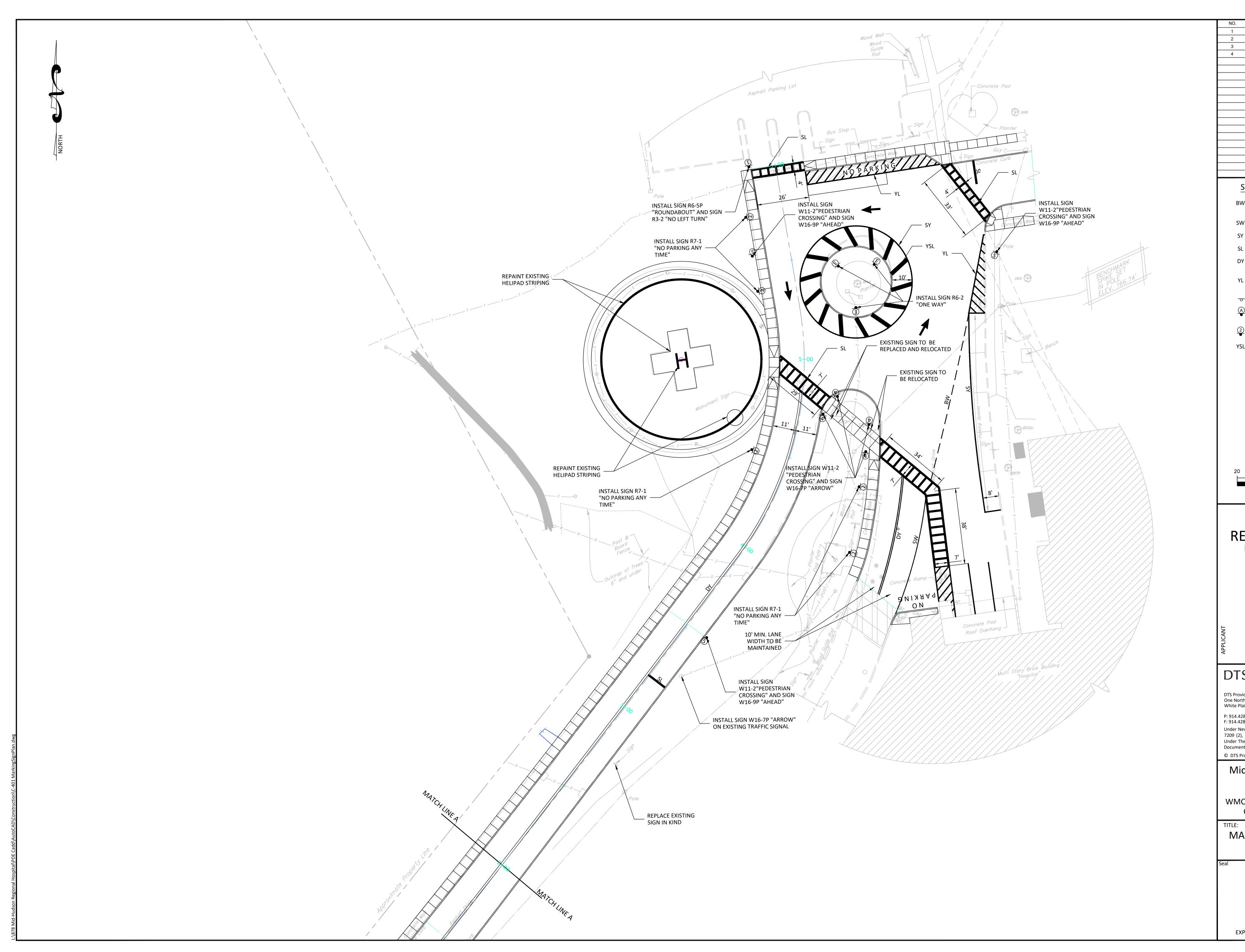
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2. ALL WORK ZONE TRAFFIC CONTROL SHALL MEET THE MOST CURRENT NYSDOT STANDARDS AND SPECIFICATIONS INCLUDING BUT NOT LIMITED TO NYSDOT SPECIFICATIONS SECTION 619

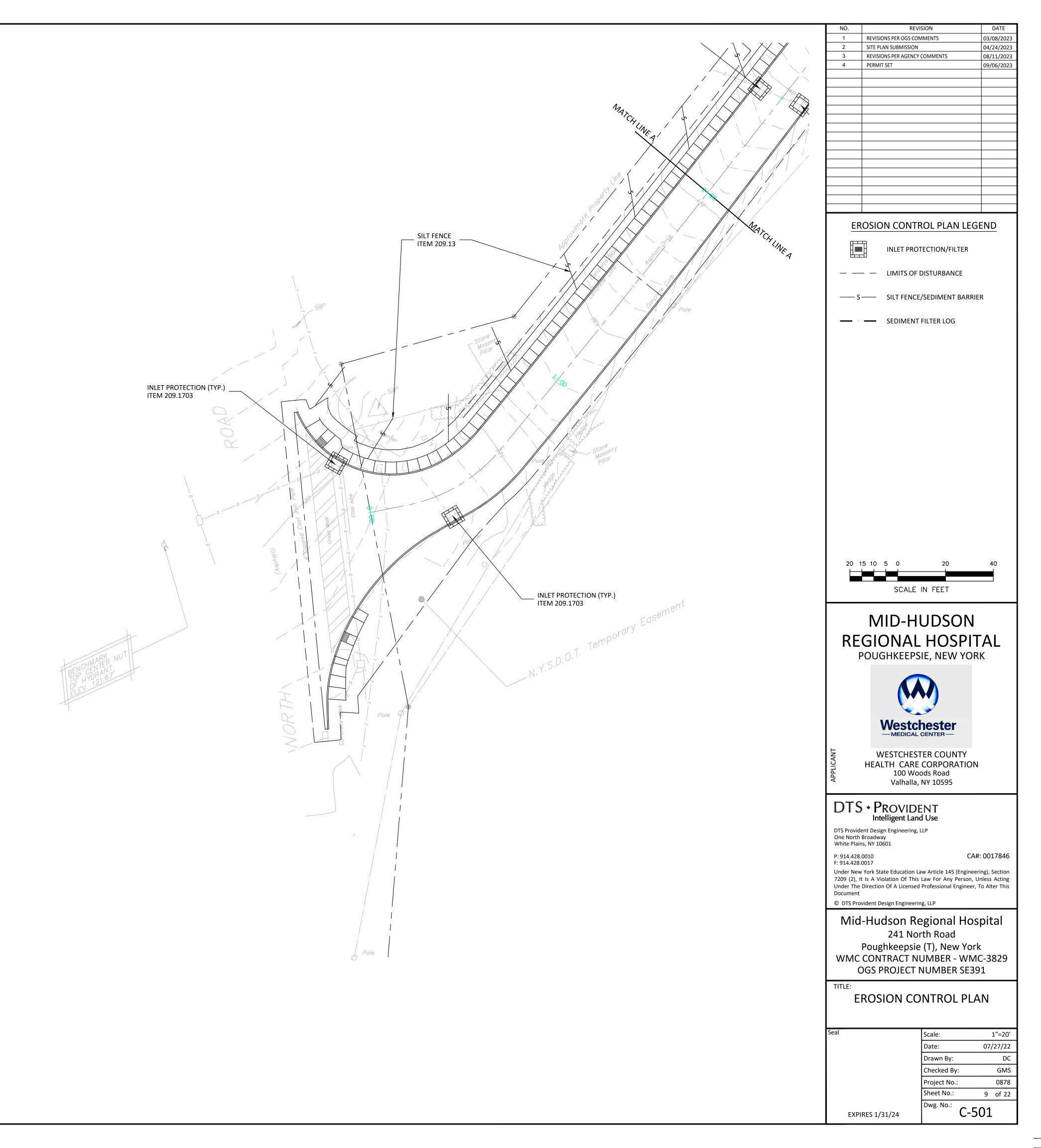


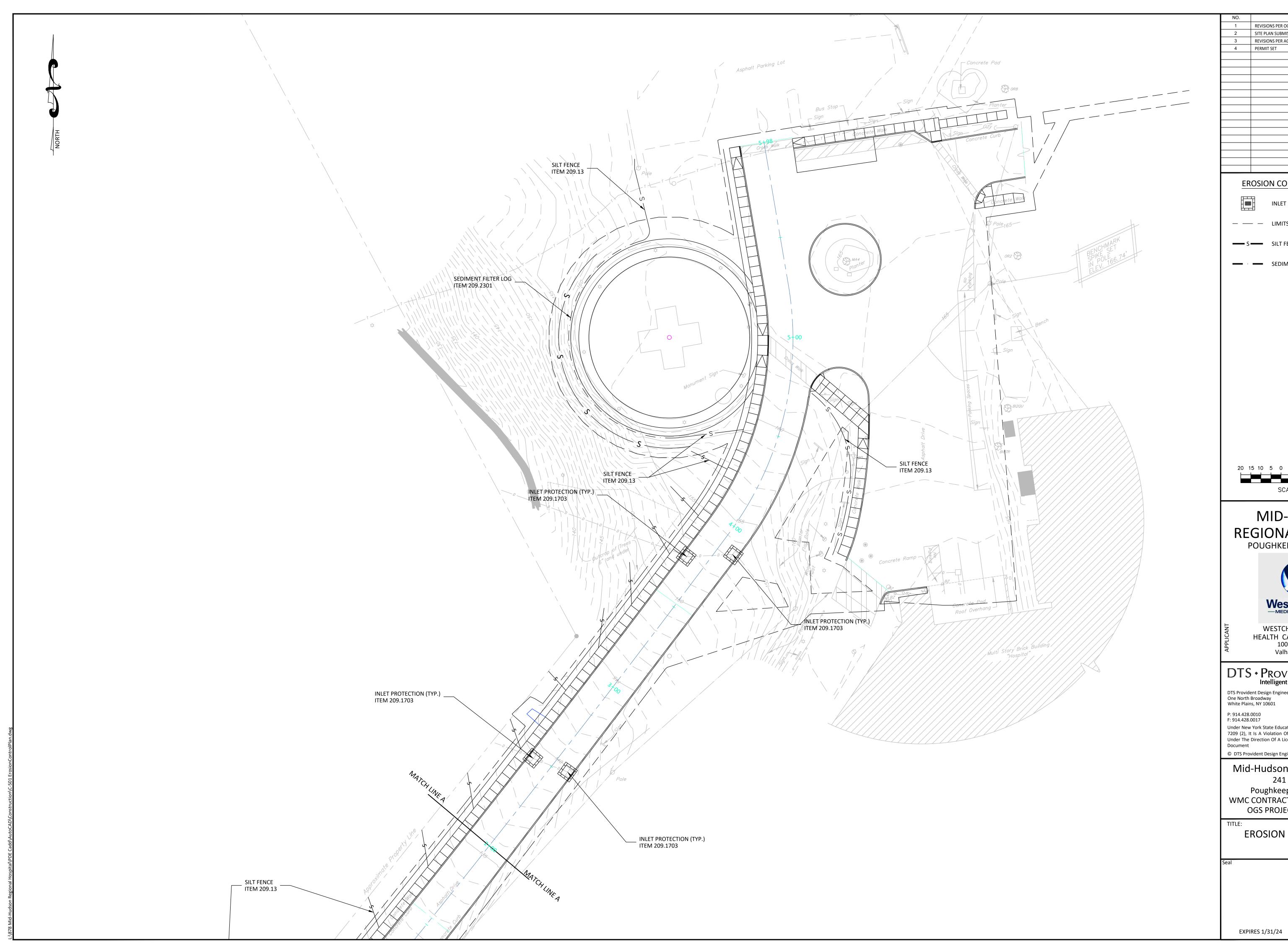


NO.	PE	VISION	DATE
1	SITE PLAN SUBMISSIO		04/24/2023
2	REVISIONS PER TOWN REVISIONS PER AGENO		06/22/2023 08/11/2023
4	PERMIT SET		09/06/2023
S	TRIPING & SIG	GN PLAN LEGEI	ND
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		E/30' SPACE)	
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SY	SOLID YEL	LOW LANE/EDGE LIN	IE, 4"
SL	SOLID WH	ITE LINE, 12" WHITE	
DY		UBLE YELLOW LINE LLOW LINES	
YL		LOW HATCH LINE	
		z, 3'-0" O.C. @ 45°)	
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A	EXISTING	SIGN TO BE REMOVE	D
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20 1	SCALE	IN FEET	40
	SCALE	UDSON	
	SCALE	IN FEET	
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RE	SCALE MID-H GIONA	UDSON HOSPIT	TAL
RE	SCALE MID-H GIONA	UDSON HOSPIT	TAL
RE	SCALE MID-H GIONA	UDSON HOSPIT	TAL
RE	SCALE MID-H GIONA POUGHKEEP	UDSON HOSPIT SIE, NEW YOR	TAL
RE	SCALE MID-H GIONA POUGHKEEP	UDSON HOSPIT	TAL
RE	SCALE MID-H GIONA POUGHKEEP	UDSON HOSPIT SIE, NEW YOR	TAL
RE	SCALE MID-H GIONA POUGHKEEP	UDSON HOSPIT SIE, NEW YOR	AL
RE	SCALE MID-H GIONA POUGHKEEP OUGHKEEP WESTCHES HEALTH CAR 100 W	UDSON HOSPI SIE, NEW YOR	AL
APPLICANT	SCALE MID-H GIONAL POUGHKEEP OUGHKEEP WESTCHES HEALTH CAR 100 W Valhalla	IN FEET	AL
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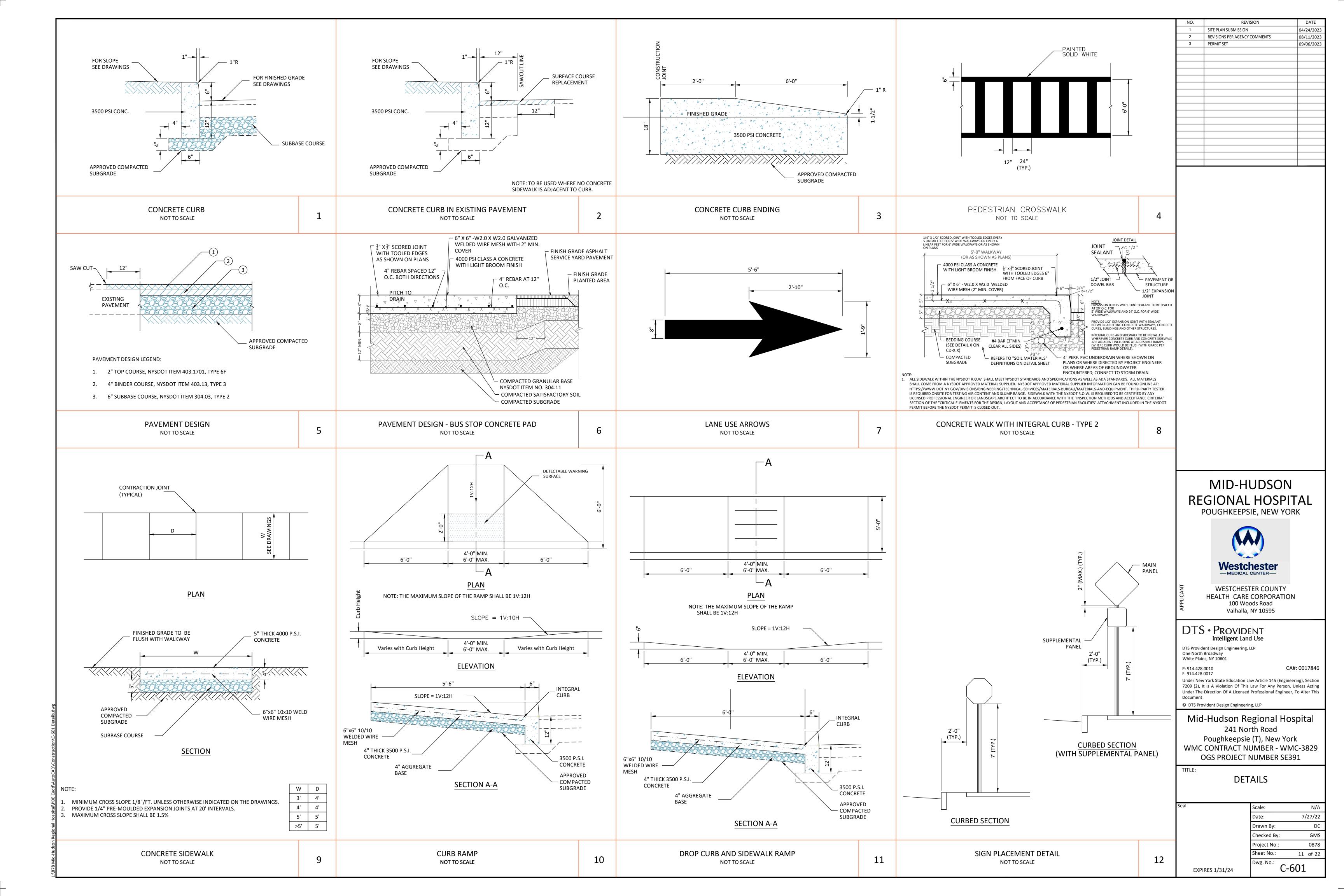
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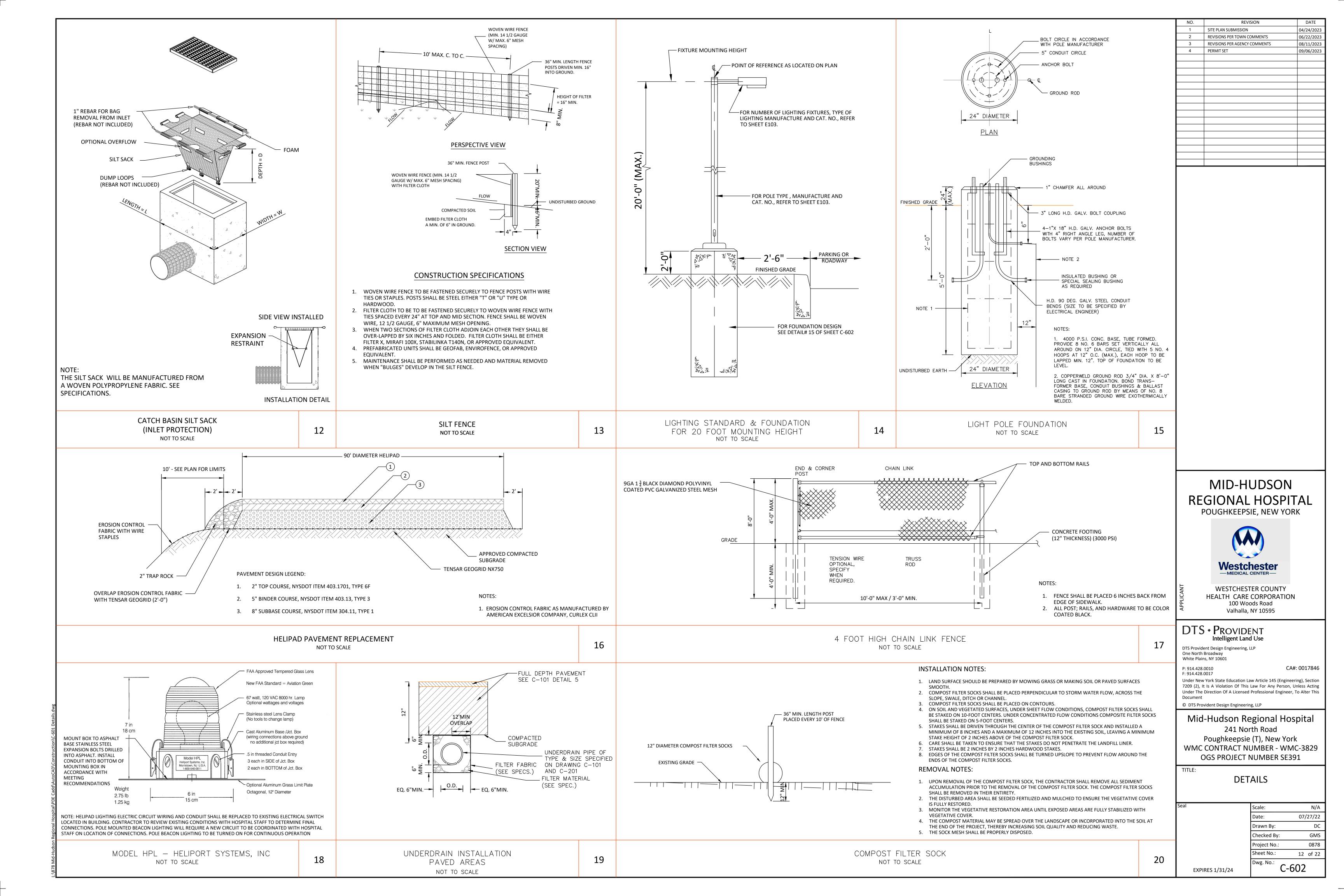






		REVISION	I DATE
NO. 1	REVISIONS	PER OGS COMMENTS	03/08/2023
2	SITE PLAN S	SUBMISSION	04/24/2023
3	REVISIONS	PER AGENCY COMMENTS	08/11/2023
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STANDARD CONDITION AND OBLIGATION FOR HIGHWAY WORK PERMITS WINTER WORK

	RESIDENCY	PERMIT	INSPE
COMPAN	IFS / CONTR	ACTORS	ARF R

DUE TO POSSIBLE SNOW FALL AND HENCE SNOW PLOWING OPERATIONS, ANY STEEL PLATES USED TO COVER AN EXCAVATION SHALL BE: (STEEL PLATES MAY BE ALLOWED FOR UP TO 14 DAYS - TIME EXTENSIONS REQUIRE EXPLANATIONS)

- A. RECESSED INTO THE PAVEMENT AND PINNED.
- APPROPRIATE DISTANCE BEFORE THE EXCAVATION; SIGNS AND LIGHTS MUST MEET NYSDOT MUTCD SPECIFICATIONS.

- ALL PERMIT WORK UNDER CONSTRUCTION BETWEEN THE DATES OF NOVEMBER 1 THROUGH MAY 1 SHALL CONFORM TO THE FOLLOWING REQUIREMENTS
- 1. GRANULAR OR OTHER FROST SUSCEPTIBLE MATERIAL SHALL NOT BE PLACED WHEN THE TEMPERATURE IS BELOW 32 DEGREES FAHRENHEIT.
- FROZEN MATERIAL SHALL NOT BE INCORPORATED INTO EMBANKMENTS OR BACKFILLS. 3. MATERIAL SHALL NOT BE PLACED ON FROZEN GROUND. 4. THE MATERIAL SHALL BE COMPACTED IN ACCORDANCE WITH HIGHWAY DESIGN MANUAL CHAPTER 13 APPENDIX 13C, "REQUIREMENTS FOR THE DESIGN AND CONSTRUCTION OF UNDERGROUND UTILITY INSTALLATIONS WITHIN THE STATE HIGHWAY RIGHT-OF-WAY" (AKA BLUE BOOK), LATEST REVISION, OR EI 04-015 (ENGINEERING INSTRUCTION)
- 5. ANY SPECIAL CONDITIONS AND RESTRICTIONS AS MAY BE IMPOSED BY THE RESIDENT ENGINEER OR THE REGIONAL GEOTECHNICAL (SOILS) ENGINEER.

TEMPORARY PAVEMENT REPLACEMENT.

ALLOWS NYSDOT STANDARD SPECIFICATIONS TO BE MET

THERE MAY BE DIFFERENT MATERIAL REQUIREMENTS, PAVEMENT / SUBBASE COURSES AND TOTAL PAVEMENT /

- STATE HIGHWAY TRAVEL AND TURNING LANES PAVEMENT STATE HIGHWAY SHOULDER PAVEMENT
- 3. COMMERCIAL DRIVEWAY ENTRANCES TO A STATE HIGHWAY A. IF MINOR COMMERCIAL DRIVEWAY ENTRANCE HAS MORE PAVEMENT THAN SHOULDER, COMMERCIAL DRIVEWAY PAVEMENT IS USED FOR DRIVEWAY APRON / TURNING AREA ACROSS THE SHOULDER.
- B. MAJOR COMMERCIAL DRIVEWAY ENTRANCES TYPICALLY HAVE THE SAME AMOUNT OF PAVEMENT AS STATE HIGHWAY TRAVEL AND TURNING LANES DUE TO VOLUME OF TRAFFIC AND TRUCK TRAFFIC USING THE ENTRANCE. 4. PUBLIC ROAD AND SUBDIVISION ENTRANCES TO A STATE HIGHWAY
- SEE COMMENTS FOR COMMERCIAL DRIVEWAY ENTRANCES SMALL SUBDIVISIONS MAY BE SIMILAR TO MINOR COMMERCIAL RESIDENTIAL DRIVEWAY ENTRANCES TO A STATE HIGHWAY
- IF SHOULDER HAS MORE PAVEMENT THAN RESIDENTIAL DRIVEWAY ENTRANCE, SHOULDER PAVEMENT IS USED FOR SHOULDER AREA AND RESIDENTIAL DRIVEWAY PAVEMENT BEGINS AT BACK EDGE OF SHOULDER.

(AOBF).

AND EI 04-015 DRAWINGS:

9WG. 7	BACKFILL AND PAVEMENT STRUCTURE REP FOR FULL DEPTH REPAIR OF UTILITY CUTS IN EXISTING PORTLAND CEMENT CONCRET
9WG. 8	BACKFILL AND PAVEMENT STRUCTURE REP FOR FULL DEPTH REPAIR OF UTILITY CUTS IN EXISTING HOT MIX ASPHALT (HMA) PAVE
9WG. 9	BACKFILL AND PAVEMENT STRUCTURE REP FOR UTILITY KEYHOLE AND BELLHOLE CUTC
9WG. 10	PLACING "CONTROLLED LOW STRENGTH M BACKFILL AT EXISTING UTILITIES
ERMITTEE MUST	MAKE ARRANGEMENTS WITH THE NYSDOT

 EMERGENCY WORK WORK WITHIN LONG-TERM STATIONARY LANE CLOSURES SAFETY WORK THAT DOES NOT ADVERSELY IMPACT TRAFFIC MOBILITY AND HAS BEEN AUTHORIZED BY THE OFFICE **OF TRAFFIC SAFETY & MOBILITY**

CONSTRUCTION ACTIVITIES THAT WILL RESULT IN TEMPORARY LANE CLOSURES SHALL BE SUSPENDED TO MINIMIZE TRAVEL DELAYS ASSOCIATED WITH ROAD WORK FOR MAJOR HOLIDAYS AS FOLLOWS:

- NEW YEAR'S DAY MONDAY XXXX BEGINNING 6:00 AM FRIDAY, DECEMBER 29 AND ENDING 6:00 AM TUESDAY, JANUARY 2
- MEMORIAL DAY MONDAY MAY 28. BEGINNING 6:00 AM FRIDAY, MAY 25 AND ENDING 6:00 AM TUESDAY, MAY 29. INDEPENDENCE DAY - WEDNESDAY JULY 4. BEGINNING 6:00 AM TUESDAY, JULY 3 AND ENDING 6:00 AM THURSDAY, JULY 5.
- LABOR DAY MONDAY, SEPTEMBER 3. BEGINNING 6:00 AM FRIDAY, AUGUST 31 AND ENDING 6:00 AM TUESDAY, SEPTEMBER 4.
- THANKSGIVING DAY THURSDAY, NOVEMBER 23. BEGINNING 6:00 AM WEDNESDAY, NOVEMBER 22 AND ENDING 6:00 AM MONDAY, NOVEMBER 27.
- CHRISTMAS DAY MONDAY, DECEMBER 25. BEGINNING 6:00 AM FRIDAY, DECEMBER 22 AND ENDING 6:00 AM
- TUESDAY, DECEMBER 26.

SHEET NUMBER	NYSDOT STANDARD SHEET TITLE
203-01	CONSTRUCTION DETAILS UNSUITABLE MATERIAL EXCAVATION AND BACKFILL
203-02	EARTHWORK TRANSITION AND BENCHING DETAILS
203-04	INSTALLATION DETAILS FOR REINFORCED CONCRETE PIPES
203-05	INSTALLATION DETAILS FOR CORRUGATED AND STRUCTURAL PLATE PIPE, PIPE ARCHES AND PLASTIC PIPES
204-01	CONTROLLED LOW STRENGTH MATERIAL (CLSM) INSTALLATION DETAILS FOR CIRCULAR AND ELLIPTICAL CORRUGATED METAL PIPES, STRUCTURAL PLATE PIPES AND PIPE ARCHES, AND REINFORCED CONCRETE AND OTHER "RIGID" PIPES
209-01	LINEAR MEASURES
209-03	DRAINAGE STRUCTURE INLET PROTECTION (2 SHEETS)
209-07	SEDIMENT TRAPS
402-01	HOT MIX ASPHALT OVERLAY SPLICE (PAVEMENT TERMINATION DETAIL)
502-01	METAL REINFORCEMENT FOR CONCRETE PAVEMENT
605-01	POROUS CONCRETE PIPE UNDERDRAIN
603-01	REINFORCED CONCRETE PIPE END SECTIONS AND CONCRETE COLLARS
608-01	SIDEWALK CURB RAMP DETAILS
609-01	CONCRETE AND STONE CURB AND GUTTER CURB
619-001	TEMPORARY POSITIVE BARRIER (TOTAL OF 7 SHEETS)
619-002	TYPE III CONSTRUCTION BARRICADES (TOTAL OF 2 SHEETS)
619-010	WORK ZONE TRAFFIC CONTROL - GENERAL NOTES
619-011	WORK ZONE TRAFFIC CONTROL - GENERAL TABLES AND LEGEND
619-012	SIGN TABLE (TOTAL OF 2 SHEETS)
619-021	WORK BEYOND SHOULDER
619-407	2-LANE 2-WAY ROADWAY LANE CLOSURE WITH FLAGGERS
619-410	2-LANE 2-WAY ROADWAY SHOULDER CLOSURE
619-421	2-LANE 2-WAY ROADWAY FLAGGING OPERATION AT INTERSECTION
619-519	2-LANE 2-WAY ROADWAY SIDEWALK DETOUR OR DIVERSION
619-520	2-LANE 2-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR
645-01	STANDARD SIGN BLANK DETAILS (TOTAL OF 2 SHEETS)
645-03	POSITIONING OF TRAFFIC SIGNS (TOTAL OF 2 SHEETS)
645-10	MULTIPLE POST SIGN INSTALLATION USING TYPE B SIGN POSTS
645-11	BI-DIRECTIONAL BREAKAWAY BASE AND HINGE ASSEMBLY
645-12	OMNI-DIRECTIONAL BREAKAWAY BASE AND HINGE ASSEMBLY
645-14	POLE-MOUNTED SIGNS
655-01	RECTANGULAR GRATES
655-02	PARALLEL BAR FRAMES AND GRATES
655-04	RETICULINE GRATES
655-06	PROOF LOADED CAST STEEL OR IRON MANHOLE FRAMES, GRATES AND COVERS
685-01	PAVEMENT MARKING DETAILS (TOTAL OF 9 SHEETS)

THE NYSDOT STANDARD CONSTRUCTION DETAILS BELOW ARE HEREIN INCORPORATED INTO THIS CONTRACT

ANY EXCAVATION WHICH EXCEEDS FIVE FEET IN DEPTH AND HAS LIVE TRAFFIC OR UTILITIES WITHIN A 1V:1H PROJECTION FROM THE BOTTOM OF THE EXCAVATION SHALL UTILIZE A SHEETING/SHORING SYSTEM WHICH PROVIDES DIRECT CONTACT AND SUPPORT OF THE EXCAVATION SIDES (A TRENCH BOX DOES NOT MEET THESE REQUIREMENTS). THE CONTRACTOR SHALL PROVIDE VERIFICATION (I.E., MANUFACTURER'S DATA SHEETS AND/OR P.E. DESIGN COMPUTATIONS) TO THE NYSDOT DEMONSTRATING THAT THE SYSTEM CHOSEN CAN ACCOMMODATE THE ANTICIPATED SOIL, WATER, TRAFFIC, AND SURCHARGE LOADINGS.

STANDARD NOTES (NYS DOT HIGHWAY DESIGN MANUAL, §21.3.9.2-A.10, DATED MAY 31, 2018)

"THE LATEST REVISIONS OF THE STANDARD SHEETS MAINTAINED BY THE DEPARTMENT, WHICH ARE CURRENT ON THE DATE OF ADVERTISEMENT FOR BIDS, SHALL BE CONSIDERED TO BE IN EFFECT. ALL PAY ITEMS AND WORK CONTAINED IN THE CONTRACT AND ANY ADDITIONAL PAY ITEMS AND WORK ENCOUNTERED DURING THE COURSE OF THE CONTRACT SHALL BE SUBJECT TO THE APPLICABLE STANDARD SHEET(S) UNLESS OTHERWISE SPECIFIED IN THE CONTRACT DOCUMENTS."

"ALL WORK CONTEMPLATED UNDER THIS CONTRACT IS TO BE COVERED BY AND IN CONFORMITY WITH THE STANDARD SPECIFICATIONS (US CUSTOMARY/METRIC) REFERENCED IN THE CONTRACT PROJECT "PROPOSAL" EXCEPT AS MODIFIED BY THESE PLANS OR BY CHANGES SET FORTH IN THE CONTRACT PROJECT "PROPOSAL".'

HELIPAD RESTORATION - SCOPE OF WORK

NYSDOT STANDARD SHEETS

DOCUMENT

- REMOVE EXISTING PAVEMENT TO SUBGRADE. INSTALL NEW CONDUIT TO LIGHTS AND STUB UP TO FIXTURE LOCATIONS.
- NOTIFY GEOTECHNICAL ENGINEER TO INSPECT TEST HOLES TO BE PERFORMED BELOW SUBGRADE. PROVIDE UP TO FOUR (4) TEST PITS. ALL TEST HOLES AND ANY VOIDS IDENTIFIED SHALL BE BACKFILLED WITH NYSDOT ITEM 304.11 OR AS DIRECTED BY ENGINEER.
- INSTALL TENSAR FABRIC OVER COMPACTED SUBGRADE PER MANUFACTURES RECOMMENDATIONS. INSTALL CURLEX CLII EROSION CONTROL FABRIC AND STAPLE INTO SOIL. OVERLAP FABRIC ON TENSAR FABRIC 2'.
- PLACE 5" NYSDOT ITEM 304.11. ADJUST SEWER MANHOLE CASTINGS TO FINISHED GRADE. REPARGE INNER MANHOLE UPPER CASTING TO MANHOLE.
- PLACE ASPHALT PAVEMENT.
- 9. PLACE STONE AROUND EDGES AS SHOWN.
- 10. INSTALL PAVEMENT MARKINGS. 11. INSTALL NEW LIGHT FIXTURES.

WINTER TIME WORK OPERATIONS **REQUIRE PRIOR PERMISSION FROM** PECTOR OR RESIDENT ENGINEER

- PERMITTEES / UTILITY COMPANIES / CONTRACTORS ARE RESPONSIBLE FOR OBTAINING ALL NECESSARY REFERENCE DOCUMENTS, PUBLICATIONS AND DRAWINGS. THEY ARE AVAILABLE ON-LINE AT WWW.DOT.STATE.NY.US
 - SNOW PLOWING REQUIREMENTS

B. PINNED WITH ASPHALT RAMPS PLACED ALONG ALL EDGES. RAMPS SHALL BE SLOPED AT 1 INCH RISE PER 6 FEET RUN MAXIMUM. "RAISE PLOW" SIGNS SUPPLEMENTED WITH TYPE A FLASHING LIGHTS MUST BE ERECTED AT AN

- WINTER TIME EARTHWORK REQUIREMENTS
- **PAVEMENT REPLACEMENT REQUIREMENTS**
- SINCE ASPHALT PLANTS ARE CLOSED IN THE WINTER, TEMPORARY PAVEMENT REPLACEMENTS SHALL BE AS PER THE NYSDOT RESIDENCY PERMIT INSPECTOR OR RESIDENT ENGINEER. PERMITTEE MUST MONITOR AND MAINTAIN THE
- TEMPORARY PAVEMENT REPLACEMENTS ARE REQUIRED TO BE REPLACED IN-KIND IN THE SPRING WHEN THE WEATHER
- SUBBASE THICKNESS FOR THE DIFFERENT PAVEMENT AREAS. ALL AREAS ARE REQUIRED TO BE REPLACED IN-KIND AS PER THE NYSDOT RESIDENCY PERMIT INSPECTOR OR RESIDENT ENGINEER. TYPES OF PAVEMENT AREAS ARE AS FOLLOWS:
- ALL PAVEMENT CROSS SLOPES AND DRIVEWAY PROFILES SHALL BE MAINTAINED OR CAN BE CORRECTED (DEPENDING ON EXTENT OF THE UTILITY WORK) AS ORDERED BY THE NYSDOT RESIDENCY PERMIT INSPECTOR OR RESIDENT ENGINEER
- PERMANENT PAVEMENT REPLACEMENTS FOR UNDERGROUND UTILITIES SHALL BE AS PER NYSDOT HIGHWAY DESIGN MANUAL CHAPTER 13 APPENDIX 13C. "REQUIREMENTS FOR THE DESIGN AND CONSTRUCTION OF UNDERGROUND UTILITY INSTALLATIONS WITHIN THE STATE HIGHWAY RIGHT-OF-WAY" (AKA BLUE BOOK), LATEST REVISION OR EI 04-015
 - REPLACEMENT DETAILS
 - CRETE (PCC) PAVEMENT
 - REPLACEMENT DETAILS UTS
 - PAVEMENT
 - REPLACEMENT DETAILS CUTOUTS
 - TH MATERIAL (CLSM)"
- IE NYSDOT RESIDENCY PERMIT INSPECTOR OR RESIDENT ENGINEER FOR FIELD VISIT TO ENSURE PAVEMENT REPLACEMENT IS DONE TO CORRECT THICKNESS.

SPECIAL NOTE TEMPORARY LANE CLOSURE RESTRICTIONS FOR MAJOR HOLIDAYS

- THERE SHALL BE NO TEMPORARY LANE CLOSURES ON ROADWAY FACILITIES OWNED AND/OR MAINTAINED BY NYSDOT ON THE MAJOR HOLIDAYS LISTED BELOW. EXCEPTIONS CAN ONLY BE MADE UNDER THE FOLLOWING CONDITIONS:

- A. GENERAL NOTES 1. CONTRACTOR TO CONTACT DUTCHESS COUNTY DEPARTMENT OF EMERGENCY RESPONSE (P.O.C. JOHN MAHONEY 845-486-2080) PRIOR TO:
 - START OF CONSTRUCTION CLOSURE OF TRAVEL LANES
 - RELOCATION OF HELIPAD
 - 2. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS. REVIEW AND APPROVAL OF THE TRAFFIC CONTROL PLAN BY THE FAIRVIEW FIRE DEPARTMENT AND DUTCHESS

MAINTENANCE AND PROTECTION OF TRAFFIC GENERAL NOTES

- COUNTY EMERGENCY RESPONSE. 4. THE CONTRACTOR MUST SUBMIT TO THE ENGINEER, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORKING DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
- THE CONTRACTOR SHALL PROVIDE THE ENGINEER, IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR. MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE. B. ON-SITE GENERAL NOTES
- 1. THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS
- 2. THE CONTRACTOR MUST SUBMIT TO THE HOSPITAL, IN WRITING, PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN FOR REVIEW AND APPROVAL PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE HOSPITAL FOR APPROVAL PRIOR TO IMPLEMENTATION OF SUCH REVISIONS
- 3. THE CONTRACTOR SHALL PROVIDE THE HOSPITAL, IN WRITING, WITH THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPAIRS OUTSIDE NORMAL WORKING HOURS. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.
- 4. THE CONTRACTOR SHALL MAINTAIN ONE DRIVE LANE OPEN AT ALL TIMES ON THE MAIN DRIVEWAY. THEY SHALL MAKE ALL PRECAUTIONS AND ADHERE TO MUTCD AND TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS TO SAFELY PROVIDE TWO-WAY OPERATIONS DURING LANE CLOSURES.
- C. ACTIVITY AREA
 - THE CONTRACTOR SHALL MAINTAIN A MINIMUM 500' LONGITUDINAL DISTANCE BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS.
- D. SIGNS THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL BY THE ENGINEER.
 - ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
 - SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT.
 - 4. ALL WARNING AND REGULATORY SIGNS SHALL BE POSTED ON BOTH SIDES OF MULTI-LANE DIVIDED HIGHWAYS, MULTI-LANE RAMPS, AND ONE -WAY STREETS. IN CASES WHERE LANE RESTRICTIONS REDUCE THE TRAVEL LANE TO ONE LANE, SIGNS SHALL BE POSTED ON THE RIGHT SIDE OF THE ACTIVE TRAVEL LANE, UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER.
- SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET. LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
- 6. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE. NYR9-12 MAY BE USED IN PLACE OF NYR9-11.
- D. CHANNELIZING DEVICES WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY.
- DRUMS SHALL BE USED FOR ANY LANE CLOSURES OR SHOULDER CLOSURES WHICH ARE TO REMAIN OVERNIGHT. E. PUBLIC ACCESS
- PROPERTY OWNERS WHOSE DRIVEWAYS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED BY THE CONTRACTOR AT LEAST 24 HOURS PRIOR TO RESTRICTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS PROPERTIES, AT LEAST ONE DRIVEWAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL BE RESTORED TO ALL DRIVEWAYS AS SOON AS POSSIBLE SUITABLE RAMPS SHALL BE INSTALLED TO MAINTAIN SMOOTH TRANSITIONS FROM RESIDENTIAL AND
- COMMERCIAL DRIVEWAYS TO AND FROM THE WORK AREA. LANE CLOSURES
- 1. THE CONTRACTOR SHALL LOCATE LANE CLOSURES TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE CURVES AND CRESTS, TO THE EXTENT CONDITIONS PERMIT. THE ENGINEER MAY REQUIRE THAT ALL LANES BE RE-OPENED AT ANY TIME IF THE ROUTE IS NEEDED FOR
- EMERGENCY PURPOSES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS OUTSIDE THE CONTRACT LIMITS. LANE WIDTHS 1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL
- BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'. THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10' THE CONTRACTOR SHALL PROVIDE A WRITTEN NOTICE TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS
- IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY. SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER. BARRIER/SHADOW VEHICLES Н.
- BARRIER AND SHADOW VEHICLES SHALL BE REQUIRED AS PER STANDARD SHEET TITLED "WORK ZONE TRAFFIC CONTROL LEGEND AND TABLES". 2. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE BARRIER
- OR SHADOW VEHICLE AND THE ACTIVE WORK AREA (ROLL AHEAD DISTANCE). THE CONTRACTOR MAY BE REQUIRED TO PROVIDE A BARRIER VEHICLE IN CONJUNCTION WITH POLICE PRESENCE IN THE WORK ZONE, TO BE INCLUDED IN THE UNIT BID PRICE FOR BASIC WORK ZONE TRAFFIC CONTROL
- CONSTRUCTION TRAFFIC MANAGEMENT PLAN AT LEAST ONE LANE OF TRAFFIC ALLOWING FOR TWO-WAY TRAVEL IS TO BE MAINTAINED AT ALL TIMES. THE CONSTRUCTION WORK IS TO OCCUR AT APPROXIMATELY 200-FOOT SECTIONS. FLAG PERSONNEL ARE TO BE POSTED AT BOTH ENDS OF THE ACTIVE CONSTRUCTION SECTION. AMBULANCES ARE TO BE GIVEN IMMEDIATE PRIORITY AS SOON AS THE OPEN LANE CAN BE CLEARED. EMERGENCY ROOM ACCESS IS TO BE MAINTAINED AT ALL TIMES. PEDESTRIAN ACCESS IS ALSO TO BE MAINTAINED. THE STANDARD NYSDOT MAINTENANCE AND PROTECTION PLANS AND PROCEDURES ARE TO BE FOLLOWED.
- ALL TEMPORARY SIDEWALK AND/OR LANE CLOSURES ARE TO BE ACCOMPANIED BY APPROPRIATE CONSTRUCTION SEQUENCE PLANS PREPARED BY THE CONTRACTOR AND SUBMITTED TO THE HOSPITAL FOR APPROVAL. IN ADDITION, THE CONTRACTOR SHALL SUBMIT AND COORDINATE WITH DUTCHESS 911, THE TOWN OF POUGHKEEPSIE, AND OTHER EMERGENCY SERVICE PROVIDERS FOR REVIEW TO ENSURE VEHICULAR AND PEDESTRIAN TRAFFIC IS SAFELY AND EFFICIENTLY ACCOMMODATED DURING ANY TEMPORARY CONDITIONS.

NO STORING OF EQUIPMENT OR WORKER PARKING SHALL BE PERMITTED ALONG ANY ACCESS ROAD WITHIN THE HOSPITAL CAMPUS. THE CONTRACTOR SHALL PROVIDE A CONSTRUCTION STAGING AND PARKING PLAN TO BE REVIEWED AND COORDINATED WITH THE HOSPITAL.

(FIC).

TEMPORARY LANE CLOSURES

IT WILL BE NECESSARY TO TEMPORARILY CLOSE TRAVEL LANES IN ORDER TO PERFORM THE CONTRACT WORK. THE FOLLOWING RESTRICTIONS SHALL APPLY TO LANE CLOSURES:

A. NO LANE CLOSURES SHALL BE PERMITTED WITHOUT THE PRIOR APPROVAL OF THE ENGINEER-IN-CHARGE

B. THE STATE RESERVES THE RIGHT TO ALLOW WORK WITHIN THE CONTRACT LIMITS BY OTHERS ALONG KEY CORRIDORS. IN ORDER TO MINIMIZE INCONVENIENCE TO THE TRAVELING PUBLIC, ANY LANE CLOSURES REQUIRED WITHIN CONTRACT LIMITS MUST BE APPROVED BY THE SURFACE TRANSPORTATION CONTROLLER (STC). KEY CORRIDORS ARE DEFINED AS I-684, TACONIC STATE PARKWAY (TSP), PALISADES INTERSTATE PARKWAY (PIP), SPRAIN BROOK PARKWAY (SBP), SAW MILL RIVER PARKWAY (SMRP), HUTCHINSON RIVER PARKWAY (HRP), CROSS COUNTY PARKWAY (CCP), NYS ROUTE 17 (FUTURE I-86), I-84 (INCLUDING THE NEWBURGH-BEACON BRIDGE), I-87 (INCLUDING THE TAPPAN ZEE BRIDGE), I-287, I-95 AND I-90. THE CONTRACTOR'S ATTENTION IS DIRECTED TO THE FACT THEY ARE RESPONSIBLE TO FORWARD REQUESTS THROUGH THE EIC TO THE STC FOR LANE CLOSURE APPROVALS. THESE REQUESTS MUST BE SUBMITTED TO THE STC AT THE EMAIL ADDRESS: DOT.SM.R08.STC, A MINIMUM OF SEVEN (7) DAYS IN ADVANCE OF SCHEDULED CLOSURES. THE STC SHALL RESPOND WITHIN FOUR (4) BUSINESS DAYS OF RECEIPT OF REQUEST. ATTENTION IS ALSO DIRECTED TO THE FACT THAT, DUE TO POSSIBLE TRAFFIC CONTROL CONFLICTS, APPROVAL MAY OR MAY NOT BE GRANTED FOR THE DATE(S) REQUESTED. ANY ADDITIONAL COSTS, DELAYS OR REMOBILIZATIONS ASSOCIATED WITH THE APPROVAL PROCESS SHALL BE INCLUDED IN THE BID PRICE FOR BASIC WORK ZONE TRAFFIC CONTROL.

NO LANE CLOSURES WILL BE PERMITTED DURING NON-WORKING HOURS, UNLESS SPECIFICALLY PROVIDED ELSEWHERE IN THE CONTRACT. ON KEY CORRIDORS AS DESCRIBED IN ITEM B (ABOVE), THE REQUIRED NUMBER OF LANES BY TIME OF DAY

CHART IN THE PROPOSAL SHALL BE FOLLOWED. ON ALL OTHER ROUTES THE SAME NUMBER OF TRAVEL LANES (INCLUDING TURNING LANES), AS EXIST PRIOR TO THIS CONTRACT, SHALL BE MAINTAINED IN EACH DIRECTION DURING THE HOURS OF 7:00 A.M. TO 9:00 A.M. AND 4:00 P.M. TO 6:00 P.M. - MONDAY TO FRIDAY, INCLUSIVE.

E. IN AREAS OF MAJOR SHOPPING MALLS, THE SAME NUMBER OF TRAVEL LANES (INCLUDING TURNING LANES), AS EXIST PRIOR TO THIS CONTRACT, SHALL BE MAINTAINED IN EACH DIRECTION DURING THE HOURS OF 10:00 A.M. TO 4:00 P.M. ON SATURDAYS.

THE TIME RESTRICTIONS LISTED ABOVE CAN BE SHIFTED AS TRAFFIC CONDITIONS WARRANT. AND THE CONTRACTOR SHALL BE AWARE THAT THE EIC HAS THE ABILITY TO ORDER ADDITIONAL TIME RESTRICTIONS OF UP TO ONE HOUR PER WORK DAY TO THE TIME RESTRICTIONS LISTED ABOVE IF TRAFFIC CONDITIONS WARRANT, AT NO COST TO THE STATE. THE CONTRACTOR SHALL NOT HAVE ANY DELAY CLAIMS AGAINST THE STATE IF THE EIC ORDERS UP TO ONE HOUR OF ADDITIONAL TIME RESTRICTIONS PER DAY. THE EIC IS AUTHORIZED TO REDUCE THE ABOVE TIME RESTRICTIONS ON THE NON-KEY CORRIDORS AS TRAFFIC CONDITIONS WARRANT.

H. THE CONTRACTOR IS ALSO ADVISED THAT THE STATE RESERVES THE RIGHT TO PRECLUDE LANE CLOSURES DURING PERIODS OF INCLEMENT WEATHER, WET OR ICY PAVEMENT, REDUCED VISIBILITY, TRAFFIC ACCIDENTS OR ANY OTHER EMERGENCIES. THE STATE MAY ALTER ANY LANE CLOSURES SHOULD TRAFFIC CONDITIONS OR OTHER UNFORESEEN CIRCUMSTANCES ARISE WHICH WOULD ADVERSELY AFFECT THE TRAFFIC FLOW. THE CONTRACTOR IS ALSO ALERTED TO THE FACT THAT INCIDENT MANAGEMENT OR TRAFFIC CONDITIONS MIGHT FORCE HIS/HER CONSTRUCTION OPERATION TO STOP, EVEN DURING TIME WHERE SUCH OPERATION WOULD NORMALLY BE PERMITTED. TEN SUCH OCCURRENCES PER CALENDAR YEAR SHOULD BE TAKEN INTO CONSIDERATION AS A REASONABLE FREQUENCY OF SUCH EVENT WHEN BIDDING THIS PROJECT. THE CONTRACTOR SHALL HAVE NO CLAIM AGAINST THE STATE FOR ANY DELAYS OR EXTRA COSTS INCURRED IN COMPLYING WITH THESE RESTRICTIONS.

THE STATE MAY GRANT A WAIVER OF THESE RESTRICTIONS UPON A TIMELY RECEIPT OF A REQUEST FOR SAID WAIVER FROM THE CONTRACTOR. A MINIMUM OF FIVE (5) WORKING DAYS FOR THE REVIEW OF THE CONTRACTOR'S REQUEST WILL BE REQUIRED.

Town of Poughkeepsie Planning Board **Planning Department Notes**

The following notes shall be placed on any subdivision plat and site plan map.

1. The location, size, placement, and installation of any highway signage regulating traffic on a town road (i.e. "Stop") or advisory sign (i.e. "Stop Ahead") shall be in accordance with the Federal Highway Administration standards as set forth in the Manual on Uniform Traffic Control Devices (MUTCD), and shall be installed at the direction of the Town of Poughkeepsie Police Department in consultation with the Town of Poughkeepsie Highway Superintendent.

2. It is the responsibility of the owner/applicant to submit to the Planning Board proof that the conditions of approval have been completed, and the signature of the Planning Board Chairman shall be withheld pending receipt of a written memorandum from the Planning Department verifying that the conditions of approval have been completed.

3. Prior to commencement of any site work the applicant's contractor and consulting engineer shall meet with the Building Department, the Town Engineer, and the Planning Department to discuss the phasing of the site work and the construction work, the placement of erosion control measures, requirements for certification from the applicant's professionals, implementation of required mitigation, approval of field changes, and periodic field inspections by the Building Department, the Town Engineer, and the Planning Department.

4. At the completion of construction, and prior to issuance of a temporary or a permanent Certificate of Occupancy for any structure, the applicant shall provide to the Planning Department, the Town Engineer, and the Building Inspector a certification in the form required by the Town, prepared by a New York State Licensed Professional Engineer, a NYS Licensed Land Surveyor and other New York State licensed professionals as the case may be, that all site work has been carried out and completed in compliance with the approved plans for the project. Additionally, the applicant shall provide to the Planning Department, the Town Engineer, and the Building Inspector an "as built" survey of the completed site work including associated storm water management facilities and any easements.

5. In the event the applicant seeks a temporary or final Certificate of Occupancy for any building or use prior to the completion of construction of all elements shown on the approved project, the applicant shall propose and obtain Department of Planning and Engineering Department approval of a phased implementation plan. The phased implementation plan shall address buildings, access, utilities, parking, landscaping, lighting, pedestrian amenities, public safety and separation from continuing construction activities and any other elements or issues as deemed appropriate by the Town. The applicant shall provide the previously referenced engineer's certification and "as built" survey upon completion of all construction.

PRECONSTRUCTION MEETING - 911 SERVICES

A PRECONSTRUCTION MEETING TO REVIEW EMERGENCY RESPONSE NOTIFICATION REQUIREMENTS, CONSTRUCTION SCHEDULE, LANE CLOSURES, HELIPAD ACCESS, CONTRACTORS MPT PLAN AND PHASING PLAN WILL BE REQUIRED PRIOR TO ANY WORK.

THE MEETING SHALL INCLUDE THE TOWN OF POUGHKEEPSIE BUILDING AND ENGINEERING DEPARTMENTS, DUTCHESS COUNTY DEPARTMENT OF EMERGENCY RESPONSE, FAIRVIEW FIRE DEPARTMENT, POUGHKEEPSIE TOWN POLICE DEPARTMENT, EMS HELICOPTER PILOT REPRESENTATIVES, MID-HUDSON HOSPITAL REPRESENTATIVES AND THE CONTRACTOR.

A WRITTEN REPORT OUTLINING THE PROCEDURES TO BE UNDERTAKEN INCLUDING CONTACT INFORMATION WIL BE ISSUED TO REPRESENTATIVES ATTENDING THE MEETING.

NO.	REVISION	DATE
1	REVISIONS PER OGS COMMENTS	03/08/2023
2	SITE PLAN SUBMISSION	04/24/2023
3	REVISIONS PER TOWN COMMENTS	06/22/2023
4	REVISIONS PER AGENCY COMMENTS	08/11/2023
5	PERMIT SET	09/06/2023

MID-HUDSON **REGIONAL HOSPITAL** POUGHKEEPSIE, NEW YORK



WESTCHESTER COUNTY HEALTH CARE CORPORATION 100 Woods Road Valhalla, NY 10595

DTS • PROVIDENT

Intelligent Land Use DTS Provident Design Engineering, LLP

One North Broadway White Plains, NY 10601

P: 914.428.0010 F: 914.428.0017

EXPIRES

Under New York State Education Law Article 145 (Engineering), Section 7209 (2), It Is A Violation Of This Law For Any Person, Unless Acting Under The Direction Of A Licensed Professional Engineer, To Alter This Document

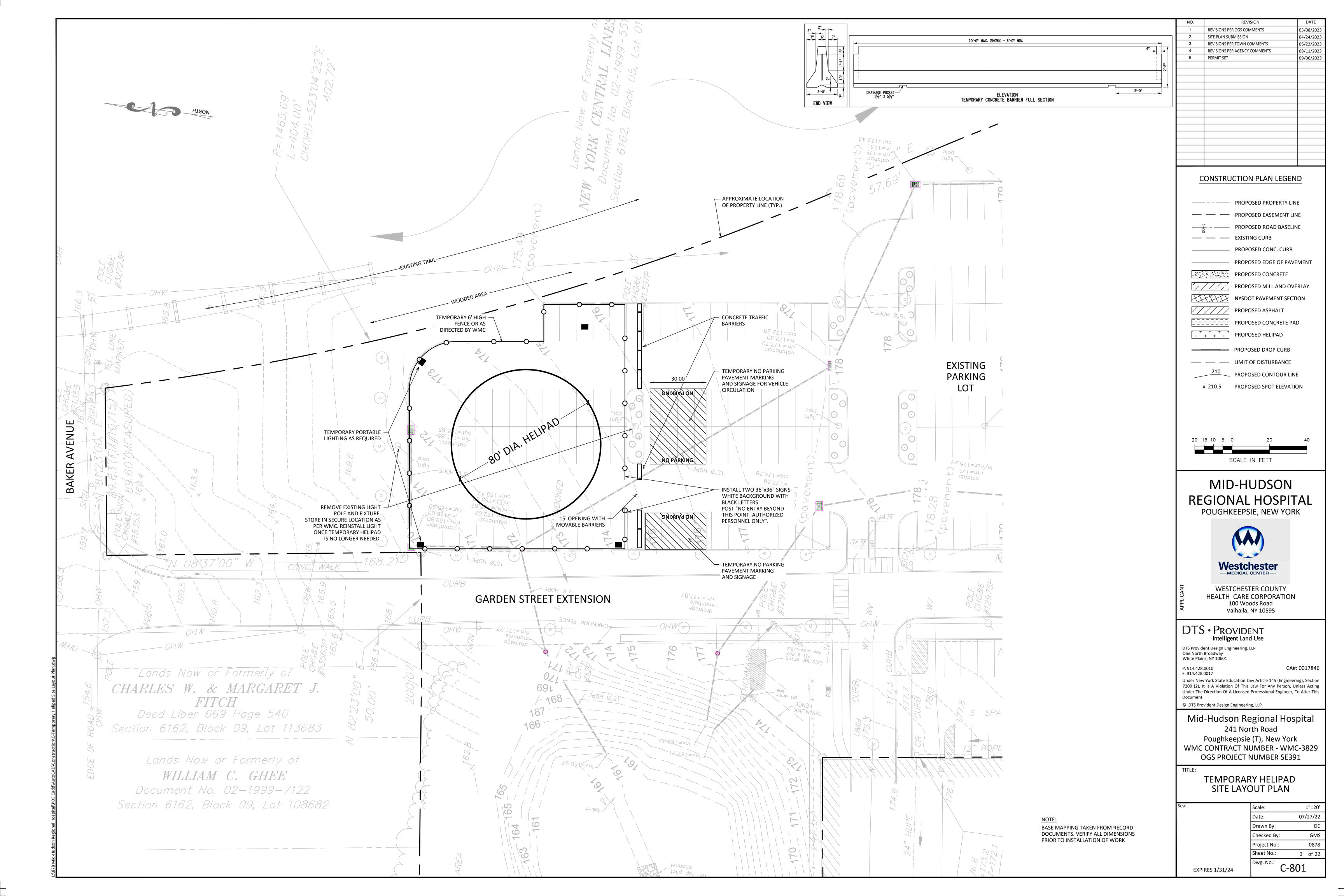
CA#: 0017846

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Mid-Hudson Regional Hospital 241 North Road Poughkeepsie (T), New York WMC CONTRACT NUMBER - WMC-3829 OGS PROJECT NUMBER SE391

TITLE: **GENERAL NOTES AND TABLES**

	Scale:	N/A
	Date:	07/27/22
	Drawn By:	DC
	Checked By:	GMS
	Project No.:	0878
	Sheet No.:	13 of 22
1/31/24	Dwg. No.: C-7	01



EMERGENCY ROOM ACCESS SHALL NOT BE CLOSED DURING CONSTRUCTION

MAINDRIVENIA

DRIVEWAY ACCESS SHALL BE MAINTAINED AT ALL TIMES. SEE CONSTRUCTION TRAFFIC MANAGEMENT PLAN.

THE R

19-94-

Sec.

AR DR FF AR AR AR

NYS ROUTE 9G

TEMPORARY HELIPAD LOCATION (SEE NOTE)

TEMPORARY PEDESTRIAN BARRIERS WITH SIGNAGE

> **TEMPORARY AMBULANCE** ROUTE TO HOSPITAL FROM TEMPORARY HELIPAD.

NOTE:

BAKER AVENUE

TEMPORARY HELIPAD SHALL INCLUDE THE FOLLOWING:

- TEMPORARY PAVEMENT MARKINGS
- TEMPORARY 6' HIGH FENCE 2.
- TEMPORARY LIGHTINGS 3.
- 4. TEMPORARY CONCRETE BARRIERS



NO.		RE	VISION	DATE				
1	REVISI PERMI	ONS PER AGENC	Y COMMENTS	08/11/2023				
		1 321						
EF	ROSIC	ON CONT	ROL PLAN	LEGEND				
			ARY FENCE/BAF					
		HELIPAD/	EMERGENCY R	OOM AREA				
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SYMBOL	ABBREVIATION	N DESCRIPTION	SYMBOL	ABBREVIATION	DESCRIPTION	SYMBOL	ABBREVIATION	DESCRIPTION	DE
	-	CONDUIT AND WIRING	200AS	-	FUSED SWITCH		KVA	KILO-VOLT-AMPERE	
·	-	CONDUIT & WIRING TO BE REMOVED UON	G	GEN	GENERATOR		KW	KILO-WATT	1. Y
— — UG— —	-	BURIED CONDUIT	 	-	GENERATOR RECEPTACLE		MAX	MAXIMUM	2.
ОН	-	OVERHEAD CONDUCTORS		GND	GROUND AS PER LOCAL CODE		МСВ	MAIN CIRCUIT BREAKER	[
	-	HOMERUN TO PANEL, ARROWS INDICATE # 1P		-	GROUND BAR		MCC	MOTOR CONTROL CENTER	3. I
 {	-	MULTI-POLE HOMERUN			GROUND ROD		MIN		4.
		ELECTRICAL EQUIPMENT AS INDICATED			TRANSFER SWITCH		MLO	MAIN LUG ONLY	
 				XFMR	TRANSFORMER				5. '
		ELECTRICAL EQUIPMENT TO BE REMOVED UON		CT	CURRENT TRANSFORMER		MTS	MANUAL TRANSFER SWITCH	
	-	ELECTRIC METER	ک	-			NIC	NOT IN CONTRACT	
	-	JUNCTION BOX	Q				NL	NIGHT LIGHT	6.
	-	FUSED DISCONNECT SWITCH		WM	WATER MAIN		NTS	NOT TO SCALE	
	-	UNFUSED DISCONNECT SWITCH	В	-	BOILER BREAK GLASS STATION		ОН	OVERHEAD	7.
\sum_{i}	-	COMBINATION MOTOR STARTER/FUSED DISC.	/ 	NC	NORMALLY CLOSED CONTACTS		Р	POLE	
\bowtie	-	MOTOR STARTER		NO	NORMALLY OPEN CONTACTS		PBO	PROVIDED BY OTHERS	
\bigwedge	-	MOTOR	K	CV	CONTROL VALVE		PNL	PANEL	8.
4_4	-	BATTERY PACK EMERGENCY LIGHT FIXTURE	LD	-	LEAK DETECTOR		PT	PRESSURE TREATED	
	-	EXIT LIGHT, FACES-SHADED, CHEVRON-ARROW	M	MD	MOTORIZED DAMPER		PVC	POLY VINYL CHLORIDE	9.
S _x	-	SINGLE POLE SWITCH		SD OR CFSD	SMOKE DAMPER		REL.	REMOVE AND RELOCATE	A.
		(x - INDICATES FIXTURE BEING CONTROLLED)		UH	UNIT HEATER		RGS	RIGID GALVANIZED STEEL	
S _x ³	-	THREE WAY SWITCH	$\begin{array}{c} \underline{P}_{\mathbf{V}} & \underline{P}_{\mathbf{V}} \end{array}$	-	SURFACE MOUNTED RACEWAY, DEVICES AS		RTU	ROOF TOP UNIT	В.
U _x		(x - INDICATES FIXTURE BEING CONTROLLED)			INDICATED		SCH	SCHEDULE	
C ⁴		FOUR WAY SWITCH		A	AMPERE(S)		SPD	SURGE PROTECTION DEVICE	
S ⁴ _x		(x - INDICATES FIXTURE BEING CONTROLLED)		AC	AIR CONDITIONER				— C.
				ACC	AIR CONDITIONER CONDENSER		SW	SWITCH(ES)	
S_x^{DIM}	_	DIMMER SWITCH (x - INDICATES FIXTURE BEING CONTROLLED)		AFF	ABOVE FINISHED FLOOR		TELCO	TELEPHONE COMPANY	
-				AF	AMPERAGE OF FUSE		TYP	TYPICAL	— D.
S _M	-	MOTOR RATED TOGGLE SWITCH		AGL	ABOVE GRADE LEVEL		UG	UNDERGROUND	
Sκ	-	KEY OPERATED SINGLE POLE SWITCH		AHU	AIR HANDLING UNIT		UON	UNLESS OTHERWISE NOTED	
S∨	-	SPEED CONTROLLER (FB0)		AL	ALUMINIUM		UV	UNIT VENTILATOR	
\bigcirc	-	WALL MOUNTED OCCUPANCY SENSOR		ARC	ARC FAULT INTERRUPTER		VIF	VERIFY IN FIELD	
\bigcirc	-	CEILING MOUNTED OCCUPANCY SENSOR		AS	AMPERAGE OF SWITCH		V	VOLT(S)	
ŧ	-	DUPLEX RECEPTACLE		ATS	AUTOMATIC TRANSFER SWITCH		VSD	VARIABLE SPEED DRIVE	
	-	DOUBLE DUPLEX RECEPTACLE		AWG	AMERICAN WIRE GAUGE		WG	WIRE GUARD	
Θ	-	SPECIAL RECEPTACLE		BCW	BARE COPPER WIRE		WH	WATER HEATER	
\bigtriangledown	-	TELEPHONE OUTLET		BLDG	BUILDING		WP	WEATHERPROOF	
▼ ×	-	DATA OUTLET (x - INDICATES # OF JACKS, 1 JACK		BMS	BUILDING MANAGEMENT SYSTEM	NOTES:			
·		UON)		С	CONDUIT			Y NOT BE APPLICABLE FOR THIS PROJECT. & LIGHT FIXTURE SYMBOLS.	
				CD	CANDELA	,			
<u>s</u>	-	SECURITY ALARM HORN PUSHBUTTON		CKT	CIRCUIT	GENERAL NO	JIE2		
				CLG	CEILING		N IS NEW UNLES	S OTHERWISE NOTED (UON) EXISTING TO	
CFA		CALL FOR AID PULL STATION				REMAIN (EX.).			
PTZ	-	SECURITY CAMERA PTZ - PAN, TILT, ZOOM		COL	COLUMN			SIDERED SCHEMATIC ONLY AND DO NOT CATIONS AND DETAILS OF THE WORK TO BE	Α
	-	CALL FOR AID A/V DEVICE		CU	COPPER	INSTALLED.			AL
				CUH	CABINET UNIT HEATER			ALL BE RESPONSIBLE FOR OBTAINING ALL ALL FEES ASSOCIATED WITH THIS WORK	1. 2.
EPO	EPO	EMERGENCY POWER OFF SWITCH		DEM.	DEMOLISH AND REMOVE		WITH THE UTILITY	COMPANY (AS REQUIRED), AND WITH LOCAL	3.
RASP	RASP	RESCUE ASSIST. SYSTEM MASTER STATION		DISC	DISCONNECT			3E RESPONSIBLE TO HIRE A THIRD PARTY	TI
F	-	REMOTE RESCUE STATION		DIM	DIMMER	ELECTRICAL INSPE	ECTION AGENCY T	O PROVIDE UL INSPECTIONS AND SUBMIT A	
O B	-	PUBLIC ADDRESS BELL		DWG	DRAWING				1.
С	-	CLOCK		ELEV	ELEVATOR	5. ALL WORK INVOL APPROVED BY THE		RIC SERVICE SHALL BE COORDINATED AND Y.	2.
ТС	-	TIME CLOCK	-	EMT	ELECTRICAL METALLIC TUBING	6. ALL CONDUCTORS	SHALL BE COPPER	R UON ON DRAWINGS.	3.
CR	-	CARD READER		EM	EMERGENCY			RICAL DRAWINGS ARE AVAILABLE TO THE	4.
DA	-	DOOR ALARM		EX.	EXISTING TO REMAIN	TO MAKE A DERIV	ATIVE WORK OF T	GRANT THE CONTRACTOR A LIMITED LICENSE THE DATABASE FOR THE PURPOSE OF SHOP	
ES	-	ELECTRIC DOOR STRIKE		F	FLOOR	ENGINEER SHALL	PROVIDE A REL	BUILT DRAWINGS. UPON REQUEST, THE EASE FORM THAT MUST BE SIGNED AND	5.
MD	-	SECURITY MOTION DETECTOR		FBO	FURNISHED BY OTHERS	RETURNED BY THE	CONTRACTOR PR	IOR TO RELEASE OF THE ELECTRONIC FILES.	
KP	-	KEY PAD		FC	FAN COIL UNIT	8. CIRCUIT NUMBERS		ATION PURPOSES ONLY. ACTUAL CIRCUIT THE FIELD.	
						9. INSTALL CONDUIT I	EXPANSION FITTIN	GS AT ALL LOCATIONS WHERE CONDUITS	
				GEN	GENERATOR				
EPO	EPO	EMERGENCY POWER OFF SWITCH		GFI	GROUND FAULT INTERRUPTER	PAINTING, AND FIN	IAL RESTORATION	L PROVIDE ALL CUTTING, PATCHING, REQUIRED TO FACILITATE THE DEMOLITION	
RASP	RASP	RESCUE ASSIST. SYSTEM MASTER STATION		HP	HORSEPOWER	TO PANELBOARDS	, CONDUITS, WIRIN	CAL EQUIPMENT, INCLUDING BUT NOT LIMITED IG, DEVICES, FIXTURES, ETC. INCLUDING	
F	-	REMOTE RESCUE STATION		HVAC	HEATING VENTILATION AIR CONDITIONING	ABOVE CEILINGS.	CONTRACTOR TO F	REMOVE AND REPLACE CEILINGS, AND OPEN DEXECUTE THE ELECTRICAL WORK.	
	СВ	CIRCUIT BREAKER		IG	ISOLATED GROUND		,		
, o	-	ENCLOSED CIRCUIT BREAKER		IMC	INTERMEDIATE METAL CONDUIT				

	NO.	REVISION	DATE
	1 2	SITE PLAN APPLICATION REVISIONS PER TOWN COMMENTS	04/24/2023
EFINITION OF TERMS	3 4	ISSUED FOR REVIEW REVISED PER TOWN AND OGS COMMENT	08/18/2023 IS 08/23/2023
WHEREVER IN THE CONTRACT DOCUMENTS THE WORD "CLIENT" IS USED, IT MUST BE UNDERSTOOD THAT "MID-HUDSON REGIONAL HOSPITAL" IS INTENDED.			
WHEREVER IN THE CONTRACT DOCUMENTS THE WORD "SITE ENGINEER" IS USED, IT MUST BE UNDERSTOOD THAT "DTS PROVIDENT" IS INTENDED.			
WHEREVER IN THE CONTRACT DOCUMENTS THE WORD "ENGINEER" IS USED, IT MUST BE UNDERSTOOD THAT "OLA CONSULTING ENGINEERS" IS INTENDED.			
WHEREVER IN THE CONTRACT DOCUMENTS THE WORDS "ELECTRICAL UTILITY" OR "POWER COMPANY" ARE USED, IT MUST BE UNDERSTOOD THAT "CENTRAL HUDSON GAS & ELECTRIC CORP" IS INTENDED.			
"WORK" MUST BE DEEMED TO CONSIST OF ALL LABOR AND OPERATIONS, TRANSPORTATION, HOISTING, MATERIALS, TOOLS, EQUIPMENT, SERVICES, INSPECTIONS, INVESTIGATIONS, COORDINATION AND SUPERVISION REQUIRED AND / OR REASONABLY NECESSARY TO PRODUCE THE CONSTRUCTION REQUIRED BY THE CONTRACT DOCUMENTS.			
"FURNISH" MEANS THE DESIGN, FABRICATION, PURCHASE AND DELIVERY TO THE JOB SITE.			
"INSTALL OR INSTALLATION" MEANS THE ACT OF PHYSICALLY PLACING, APPLYING, SETTING, ERECTING, ANCHORING, SECURING, ETC., CONSTRUCTION MATERIALS, EQUIPMENT, FURNISHINGS, APPLIANCES, AND SIMILAR ITEMS SPECIFIED AND FURNISHED AT THE JOB SITE. INSTALLATION OF SPECIFIED ITEMS MUST BE COMPLETE IN ALL RESPECTS.			
"PROVIDE" MEANS TO FURNISH AND INSTALL CONSTRUCTION MATERIAL, EQUIPMENT, ETC. AS DEFINED ABOVE.			
THE FOLLOWING ARE DEFINITIONS OF SHOP DRAWING STAMP ACTIONS:			
"NO EXCEPTIONS TAKEN" MEANS THAT THE SHOP DRAWING IS CORRECT AS TO PERFORMANCE, CAPACITY, ETC. AND SUBSTANTIAL CONFORMANCE TO THE CONTRACT DRAWINGS AND SPECIFICATIONS. FABRICATION AND/OR PURCHASE MAY COMMENCE.			
"MAKE CORRECTIONS NOTED" MEANS THAT THE SHOP DRAWING IS CORRECT AS TO PERFORMANCE, CAPACITY, ETC. AND SUBSTANTIAL CONFORMANCE TO THE CONTRACT DRAWINGS AND/OR SPECIFICATIONS, SUBJECT TO AND IN COMPLIANCE WITH THE ANNOTATIONS AND/OR CORRECTIONS INDICATED ON THE SHOP DRAWING. FABRICATION AND/OR PURCHASE MAY COMMENCE.			
"AMEND AND RESUBMIT" MEANS THAT THE COMMENTS AND/OR CORRECTION ARE SO EXTENSIVE AND IMPORTANT THAT THE REVIEWER WANTS TO SEE HOW THE COMMENTS AND/OR CORRECTIONS ARE RESOLVED PRIOR TO RELEASE FOR FABRICATION AND/OR PURCHASE. FABRICATIONS AND/OR PURCHASE MAY <u>NOT</u> COMMENCE.			
"REJECTED" MEANS THAT THE SHOP DRAWING DOES NOT COMPLY OR CONFORM TO THE CONTRACT DRAWINGS AND/OR SPECIFICATIONS. FABRICATION AND/OR PURCHASE MAY <u>NOT</u> COMMENCE.			
TYPICAL BRANCH CIRCUIT WIRING LEGEND			
2-#12 & 1-#12 GND (1-1P-20A OR 1-1P-15A CB)			
 → 3-#12 & 1-#12 GND (3P-20A OR 3P-15A CB) → 2-#12 & 1-#12 GND (2P-20A OR 2P-15A CB) 			
- 13		MID-HUDSON	_
CIRCUIT # 15 SWITCH CONTROL		GIONAL HOSF DUGHKEEPSIE, NEW Y	
CIRCUIT #			
<u>NOTES</u> : 1. EACH 120V AND 277V CIRCUIT SHALL HAVE A DEDICATED NEUTRAL CONDUCTOR. SHARED NEUTRAL HOMERUNS ARE NOT PERMITTED.			
 CONDUCTORS SHALL BE INCREASED FOR VOLTAGE DROP AND DERATING AS PER APPLICABLE ELECTRICAL CODE. FOR CIRCUITS THAT ARE BETWEEN 100' AND 150' IN 			
LENGTH, PHASE AND NEUTRAL CONDUCTORS SHALL BE #10 AWG. FOR CIRCUITS THAT ARE BETWEEN 150' AND 225' IN LENGTH, PHASE AND NEUTRAL CONDUCTORS SHALL BE #8 AWG. FOR LENGTHS GREATER THAN 225' IN LENGTH, VERIFY		Westchester — MEDICAL CENTER —	
CONDUCTOR SIZES WITH ENGINEER.	CANT	WESTCHESTER COUNTY HEALTH CARE	,
APPLICABLE CODES	APPLICANT	CORPORATION 100 Woods Road	
L WORK SHALL CONFORM WITH CURRENT CODES, INCLUDING:		Valhalla, NY 10595	
2020 NEW YORK STATE BUILDING CODE 2020 NEW YORK STATE ENERGY CONSERVATION CODE 2017 NATIONAL ELECTRIC CODE	DTS Provide	PROVIDENT Intelligent Land Use ent Design Engineering, LLP	
RENCHING NOTES		s, NY 10601	
CONTRACTOR SHALL LOCATE ALL EXISTING UNDERGROUND UTILITIES THAT ARE NOT	P: 914.428.0 F: 914.428.0 Under New		eering), Section
PART OF N.Y. STATE "CODE 753" PRIOR TO DIGGING. ALL EXCAVATING IN THE AREA OF THE EXISTING UNDERGROUND EQUIPMENT, PIPES		: Is A Violation Of This Law For Any Persor Direction Of A Licensed Professional Engined	
AND CONDUITS SHALL BE PERFORMED BY HAND. ANY AREA/PLANTS OR LANDSCAPING OR PAVEMENTS DISTURBED DURING THE		vident Design Engineering, LLP	
EXCAVATION SHALL BE RESTORED OR REPLACED TO MATCH EXISTING CONDITIONS BY THE CONTRACTOR AT NO COST TO THE OWNER.		OLA Consulting Engineers 50 Broadway, Hawthorne, NY 10532	
ANY EXISTING BURIED CONDUITS, DRAINAGE, SPRINKLER PIPING, ETC. THAT IS DISTURBED AND/OR DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED BY THE CONTRACTOR AT NO COST TO THE OWNER		914,747.2800 8 West 38th Street, Suite 501	
CONTRACTOR AT NO COST TO THE OWNER. THE PLANS SHOW SOME KNOWN SUBSURFACE STRUCTURES, ABOVE GROUND STRUCTURES AND/OR UTILITIES BELIEVED TO EXIST IN THE WORKING AREA, EXACT	CONSULTING	New York, NY 10018 646.849.4110	
LOCATION OF WHICH MAY VARY FROM THE LOCATIONS INDICATED. IN PARTICULAR, THE CONTRACTOR IS WARNED THAT THE EXACT OR EVEN APPROXIMATE LOCATION OF			CA#: 0018400
SUCH PIPELINES, SUBSURFACE STRUCTURES AND/OR UTILITIES IN THE AREA MAY OR MAY NOT BE SHOWN; AND IT SHALL BE HIS RESPONSIBILITY TO PROCEED WITH GREAT		LECTRICAL SYMBO EVIATIONS AND GE	-
CARE IN EXECUTING ANY WORK. 48 HOURS BEFORE YOU DIG, DRILL OR BLAST, CALL 1-800-272-4480 (NYC & LI) OR 1-800-962-7962 (NY STATE).		NOTES	
	Saal	DF NEW FOR Date:	04/24/2023
	* ×	Drawn By: Checked By:	JV JL
	LICEN	Project No.:	NDTS0004.00
	No.	071678 C Dwg. No.:	1 of 7
		FESSIONA RES 04/30/2024	E001

SPECIFICATIONS

E-1. SCOPE OF WORK

- A. ALL WORK SHOWN ON THE DRAWINGS IS NEW UNLESS OTHERWISE NOTED EXISTING TO REMAIN (EX.). THIS CONTRACTOR SHALL PROVIDE ALL MATERIALS, LABOR, EQUIPMENT, TOOLS, APPLIANCES, SERVICES, HOISTING, SCAFFOLDING, SUPERVISION AND OVERHEAD FOR THE FURNISHING AND INSTALLING OF ALL THE ELECTRICAL AND RELATED WORK COMPLETE, IN ACCORDANCE WITH THE DRAWINGS AND SPECIFICATIONS, INCLUDING BUT NOT LIMITED TO THE FOLLOWING:
- 1. MODIFICATION OF EXISTING PANELBOARDS, BALANCING AND UPDATED TYPED DIRECTORIES.
- 2. REMOVAL, DISPOSAL, RELOCATION AND/OR INSTALLATION OF FIRE ALARM SYSTEM COMPONENTS, ELECTRICAL LIGHTING FIXTURES, SWITCHES, RECEPTACLES, WIRING, PANELBOARDS, TRANSFORMERS, DISCONNECT SWITCHES AND ASSOCIATED CONDUIT, ALARM WIRING AND ANY OTHER ELECTRICAL EQUIPMENT.
- 3. LIGHTING FIXTURES, COMPLETE WITH NECESSARY HANGER ASSEMBLIES, STEMS AND SWIVELS, COUPLINGS, LAMP AUXILIARIES, LAMPS, MISCELLANEOUS MOUNTING DEVICES AND HARDWARE TO MEET THE BOCA SEISMIC REQUIREMENTS.
- 4. JUNCTION AND OUTLET BOXES COMPLETE WITH COVERS, SWITCHES, RECEPTACLES AND ANY OTHER WIRING DEVICES AND SPECIAL COVERPLATES.
- 5. CONDUIT, CONDUIT FITTINGS, OUTLET BOXES, JUNCTION AND PULL BOXES, TROUGHS, WIREWAYS AND ALL APPURTENANCES NECESSARY FOR ELECTRICAL RACEWAY SYSTEMS, INCLUDING NECESSARY SUPPORTS AND FASTENERS.
- 6. INSULATED CONDUCTORS COMPLETE WITH SPLICES AND CONNECTIONS, INCLUDING CONNECTORS AND CONNECTION LUGS.
- 7. GROUNDING AND BONDING SYSTEM. 8. HOLES AND SLEEVES FOR CONDUITS PASSING THROUGH WALLS, FLOORS AND PARTITIONS.
- TAGGING AND IDENTIFYING ALL EQUIPMENT AND DEVICES WITH NAMEPLATES.
- 10. FIELD TESTS OF ALL EQUIPMENT AND ITS OPERATIONS AS SPECIFIED.
- 11. CUTTING AND PATCHING AS REQUIRED FOR INSTALLATION OF ELECTRICAL WORK.
- 12. TEMPORARY POWER AND LIGHT AS REQUIRED.
- 13. AS-BUILT DRAWINGS.

E-2 MATERIAL AND WORKMANSHIP

- A. GENERAL
- 1. THE WORK PERFORMED SHALL BE "FIRST-CLASS WORK" IN EVERY RESPECT. THE WORK SHALL BE PERFORMED BY A LICENSED ELECTRICIANS SKILLED IN THEIR RESPECTIVE TRADES, WHO SHALL AT ALL TIMES BE UNDER THE SUPERVISION OF COMPETENT PERSONS.
- 2. WORK THAT IS SLIPSHOD, POORLY LAID OUT, NOT PERFECTLY ALIGNED, OR THAT IS NOT CONSISTENT WITH THE REQUIREMENTS GENERALLY ACCEPTED IN THE TRADE FOR "FIRST-CLASS WORK" SHALL NOT BE ACCEPTABLE.
- 3. IN ADDITION TO THE MATERIALS SPECIFIED ELSEWHERE, ALL OTHER MISCELLANEOUS ITEMS NECESSARY FOR THE COMPLETION OF THE WORK SHALL BE FURNISHED AND INSTALLED BY THE CONTRACTOR TO THE EXTENT THAT ALL SYSTEMS BE COMPLETE AND OPERATIVE.
- 4. ALL MATERIALS AND EQUIPMENT FURNISHED UNDER THIS SECTION SHALL BE NEW AND LISTED AND/OR LABELED BY THE UNDERWRITERS' LABORATORIES, INC., FOR THE APPLICATION, UNLESS OTHERWISE SPECIFIED HEREIN. MATERIALS, MATERIAL SIZES AND METHOD OF CONSTRUCTION NOT SPECIFIED SHALL BE AT LEAST EQUAL TO OR BETTER THAN THE STANDARDS AS LISTED BY THE UNDERWRITERS' LABORATORIES, INC., AND/OR THE REQUIREMENTS OF THE LAWS, REGULATIONS AND CODES MENTIONED HEREINAFTER. DEFECTIVE MATERIALS OR MATERIALS DAMAGED IN THE COURSE OF INSTALLATION OR TESTS SHALL BE REPLACED OR REPAIRED IN A MANNER MEETING WITH THE APPROVAL OF THE CLIENT.
- 5. ALL WORK UNDER THIS SECTION SHALL BE PERFORMED IN COOPERATION WITH THE WORK BY ALL OTHER CONTRACTORS AND SUBCONTRACTORS ON THE PROJECT, IN ORDER TO AVOID INTERFERENCES AND TO SECURE THE PROPER INSTALLATION OF ALL WORK. THIS CONTRACTOR SHALL REVIEW THE DRAWINGS AND SPECIFICATIONS COVERING THE WORK TO BE PERFORMED UNDER ALL SECTIONS, SO THAT HE UNDERSTANDS THE RELATION AND EXTENT OF THE WORK OF THIS SECTION WITH RESPECT TO THE WORK OF THE OTHER SECTIONS.
- 6. ALL WORK SHALL BE COORDINATED WITH THE OWNER & CLIENT AND SHALL MEET ALL CLIENT STANDARDS WHERE APPLICABLE AND SHALL BE SUBJECT TO APPROVAL FROM AN AUTHORIZED CLIENT REPRESENTATIVE. ALL MATERIALS USED SUCH AS CONDUIT, WIRING, LIGHT FIXTURES, WIRING DEVICES, ETC. SHALL MEET CLIENT STANDARDS UNLESS OTHERWISE INDICATED.
- E-3 LAWS, REGULATIONS AND CODES

A. GENERAL:

1. ALL WORK UNDER THIS SECTION SHALL COMPLY WITH THE APPLICABLE FEDERAL, STATE, LOCAL CODES AND AUTHORITIES. WHERE REFERENCE IS MADE TO LAWS, CODES, REGULATIONS AND STANDARDS, THESE DOCUMENTS, INCLUDING THE LATEST REVISIONS AND AMENDMENTS THERETO IN EFFECT AS OF THE DATE OF BID OPENING, SHALL FORM PART OF THESE SPECIFICATIONS.

E-4 SHOP DRAWINGS

- A. GENERAL: MANUFACTURER'S DATA OR SHOP DRAWINGS OF THE FOLLOWING APPARATUS GIVING FULL INFORMATION AS TO DIMENSIONS. MATERIALS, AND ALL INFORMATION PERTINENT TO THE ADEQUACY OF THE SUBMITTED EQUIPMENT INCLUDING WIRING DIAGRAMS SHALL ALSO BE SUBMITTED FOR APPROVAL AS DIRECTED:
- 1. CONDUIT
- 2. CONDUCTORS
- 3. WIRING DEVICES
- 4. SPECIAL OUTLETS/EQUIPMENT 5. LIGHTING FIXTURES
- 6. LIGHTING CONTROL DEVICES/SYSTEMS
- 7. PANELBOARDS
- 8. MANHOLES/HAND HOLES
- 9. COORDINATION DRAWINGS SHOWING ROUTING OF ALL CONDUITS
- E-5 RECORD DRAWINGS

A. GENERAL:

1. THE CONTRACTOR SHALL MAINTAIN AN ACCURATE RECORD OF ALL DEVIATIONS IN WORK AS ACTUALLY INSTALLED FROM WORK AS INDICATED. THIS RECORD SHALL BE UPDATED DAILY AND SHALL BE KEPT AVAILABLE AT THE SITE FOR INSPECTION. UPON COMPLETION OF THE WORK, AND BEFORE

E-6 INSTALLATION OF WORK

- A. GENERAL
- THE OWNER.
- OWNER.
- TO AVOID EXISTING EQUIPMENT & OBSTRUCTIONS
- NO COST TO THE CLIENT. ALL CORES SHALL BE FIRE SEALED. PRIOR TO INSTALLATION.

B. ELECTRIC SERVICE:

- MODIFICATIONS.
- ADDITIONAL COST TO THE CLIENT.

C. CONDUIT WORK:

- KOPR-SHIELD BEFORE MAKING UP THE CONNECTION.
- NOT EXCEED 7 FEET.
- INSTALLING THE BUSHINGS.
- CONDUCTOR SHALL BE PROVIDED IN ALL FLEXIBLE CONDUITS.
- AND SUPPORTS AS REQUIRED.
- FROM THE BUILDING STRUCTURE.
- CIRCUIT WIRING ABOVE HUNG CEILINGS AND IN PARTITIONS.
- PIPING, ETC., TO SUIT THE FIELD CONDITIONS.
- PULLBOXES AS REQUIRED.
- 90 DEGREE BENDS SHALL NOT BE PERMITTED.
- OR CEILING TILES.
- D. CABLE AND WIRING WORK:
- EXCEPT AS OTHERWISE SHOWN OR SPECIFIED.
- 2. ALL JOINTS, SPLICES AND TAPS FOR WIRING CONNECTIONS SHALL BE MADE

FINAL PAYMENT IS AUTHORIZED, MARKED PRINTS WITH SIGNED CERTIFICATION OF ACCURACY, SHALL BE DELIVERED TO THE OWNER'S REPRESENTATIVE.

1. THE CONTRACTOR SHALL BE RESPONSIBLE TO EXAMINE THE SITE AND CHECK ALL FIELD CONDITIONS. NOTIFY THE ENGINEER OF ANY CONDITION WHICH DIFFERS FROM THAT INDICATED ON THE PLAN.

2. ALL WORK SHALL BE CAREFULLY LAID OUT IN ADVANCE SO THAT UNNECESSARY CUTTING, CHANNELING, CHASING OR DRILLING OF WALLS, PARTITIONS, FLOORS, CEILINGS OR OTHER SURFACES WILL BE AVOIDED. WHERE WORK IS NECESSARY FOR THE PROPER INSTALLATION, SUPPORT OR ANCHORAGE OF RACEWAYS, OUTLETS OR OTHER ELECTRICAL WORK, IT SHALL BE CAREFULLY DONE IN SUCH A MANNER AS TO AVOID ANY DAMAGE. ALL WORK WHICH MAY BE DAMAGED SHALL BE REPAIRED TO THE SATISFACTION OF

3. ALL ELECTRICAL WORK SHALL BE PROTECTED AGAINST DAMAGE DURING CONSTRUCTION AND ANY WORK DAMAGED OR MOVED OUT OF LINE AFTER ROUGHING-IN SHALL BE REPAIRED AND RESET TO THE APPROVAL OF THE

4. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL ROUTING IN THE FIELD WITH EXISTING EQUIPMENT. PROVIDE ALL NECESSARY OFFSETS

5. CORE DRILLING OR TRENCHING THROUGH AN EXISTING FLOOR SLAB, WHEN REQUIRED, SHALL BE COORDINATED WITH THE OWNER. FLOOR SLABS SHALL BE RADAR SCANNED PRIOR TO CORE DRILLING OR TRENCHING. ALL WORK, INCLUDING CORE DRILLING, RADAR SCAN, INSTALLATION OF FIRE STOPPING, & CONDUIT/CABLE INSTALLATION SHALL BE PERFORMED DURING NON-BUSINESS HOURS AND INCLUDED IN BASE BID. USE EXTREME CAUTION DURING ANY CUTTING OPERATION TO AVOID DAMAGE TO EXISTING EQUIPMENT/SYSTEMS. ANY ITEMS DAMAGED AS A RESULT OF CORE DRILLING SHALL BE REPAIRED AT

6. CONTRACTOR SHALL VERIFY CONDUIT ROUTING WITH OWNER AND/OR CLIENT

1. ALL WORK INVOLVING THE ELECTRICAL SERVICE SHALL BE COORDINATED WITH AND APPROVED BY THE ELECTRICAL UTILITY COMPANY. THE CONTRACTOR SHALL PAY ALL FEES ASSOCIATED WITH THE ELECTRIC SERVICE

2. NOTIFY THE ELECTRIC UTILITY COMPANY IMMEDIATELY UPON AWARD OF CONTRACT TO COORDINATE ELECTRIC SERVICE MODIFICATIONS.

3. NOTIFY THE CLIENT AND OWNER IN WRITING AT LEAST TWO WEEKS IN ADVANCE OF ANY INTERRUPTION OF SERVICE IN THE BUILDING. INFORM THE CLIENT AND OWNER OF THE DURATION OF THE SHUTDOWN. ALL WORK INVOLVING A SHUTDOWN SHALL BE PERFORMED DURING PREMIUM TIME, AT NO

1. ALL THREADED JOINTS IN CONDUIT WORK SHALL BE MADE WATERTIGHT BY A COATING OF THOMAS & BETTS KOPR-SHIELD COMPOUND ON THE MALE THREADS ONLY. WHENEVER THREADS ARE CUT, THEY SHALL BE COATED WITH

2. EXPOSED CONDUIT ON CEILING SHALL BE RUN PARALLEL OR PERPENDICULAR TO WALL AND VISE VERSA TO CEILING, WHEN INSTALLED ON WALL. SECURE CONDUIT CLAMPS AND SUPPORTS TO MASONRY MATERIALS BY TOGGLE BOLT, EXPANSION BOLT OR STEEL INSERT. SPACING OF CONDUIT SUPPORTS SHALL

3. THE ENDS OF ALL CONDUIT SHALL BE CAREFULLY REAMED OUT FREE FROM BURRS BEFORE INSTALLATION AND AFTER THREADING. THE END OF EACH CONDUIT 1" AND SMALLER SHALL BE PROVIDED WHERE IT ENTERS A JUNCTION BOX, OUTLET BOX, CABINET, ETC., WITH A LOCK NUT AND BUSHINGS. FOR CONDUITS 1-1/4" AND LARGER, INSULATED BUSHINGS SHALL BE USED. IF INSULATED BUSHINGS ARE OF THE FULLY INSULATED TYPE, AN ADDITIONAL LOCK NUT SHALL BE USED INSIDE JUNCTION BOX OR CABINET BEFORE

4. FLEXIBLE SEAL-TITE CONDUIT AND SEAL-TITE FITTINGS SHALL BE USED TO CONNECT ALL MOTORS SO AS TO ISOLATE THE MOTION OR VIBRATION FROM THE RIGID CONDUIT SYSTEM AND THE BUILDING. AN EQUIPMENT GROUNDING

5. CONDUITS SHALL BE SECURELY FASTENED IN PLACE WITH STRAPS, HANGERS

6. CONDUIT IN HUNG CEILINGS SHALL BE SUPPORTED IN AN APPROVED MANNER

7. FLEXIBLE METALLIC CONDUIT OR MC CABLE SHALL BE USED FOR BRANCH

8. THE CONTRACTOR SHALL PROVIDE PULL BOXES, JUNCTION BOXES, CONDUITS, CONDUIT ELBOWS, AND OFFSETS IN CONDUIT RUNS WHICH INTERFERE WITH THE STRUCTURAL WOOD OR STEEL, MECHANICAL EQUIPMENT, DUCTWORK,

9. NO MORE THAN THREE RIGHT ANGLE BENDS SHALL BE PERMITTED IN CONDUIT BETWEEN ANY TWO TERMINATION OR PULLBOXES. PROVIDE ADDITIONAL

10. TELEPHONE SERVICE CONDUITS SHALL HAVE ONE 18"x18"x8" PULL BOX AFTER 270 DEGREES OF BENDS WITH A MAXIMUM OF 360 DEGREES OF BEND PER RUN. ALL BENDS IN CONDUIT SHALL BE SWEEPING BENDS FOR FIBER OPTIC CABLE.

11. ALL MC CABLE RUNS ABOVE HUNG CEILINGS SHALL BE SECURED TO BUILDING STRUCTURE. NO MC CABLES SHALL BE LEFT UNSUPPORTED ON DUCTWORK

12. WHERE MULTIPLE HOME RUNS ARE ROUTED TOGETHER IN THE SAME RACEWAY LONGER THAN 24 INCHES, CONDUCTORS SHALL BE INCREASED TO #10 AWG FOR UP TO EIGHT CONDUCTORS (HOT & NEUTRAL) MAXIMUM. INSTALLATION SHALL BE IN ACCORDANCE WITH THE AFOREMENTIONED CODE.

1. CONDUCTORS FOR BRANCH CIRCUITS SHALL BE OF SIZES INDICATED ON THE ELECTRICAL DRAWINGS, BUT SHALL NOT BE SMALLER THAN NO. 12 AWG WITH MATERIALS AS HEREINAFTER SPECIFIED.

- CONDUCTORS SHALL BE CONTINUOUS FROM OUTLET TO OUTLET, AND NO SPLICES OR CONNECTIONS SHALL BE MADE, EXCEPT WITHIN OUTLET BOXES, JUNCTION BOXES OR CABINETS.
- 4. THE NEUTRAL WIRE SHALL NOT BE USED AS A GROUND WIRE. THE NEUTRAL WIRE SHALL BE AN INSULATED WIRE AND SHALL BE CONNECTED TO THE GROUND SYSTEM AT ONE PLACE ONLY. THIS CONNECTION SHALL BE MADE AT THE BEGINNING OF THE SEPARATELY DERIVED SYSTEM.
- 5. TELEPHONE/DATA CABLING RUN ABOVE THE HUNG CEILING SHALL NOT BE LEFT UNSUPPORTED. ALL CABLING SHALL BE SUPPORTED FROM THE BUILDING STRUCTURE.

E. ELECTRICAL GROUNDING AND BONDING:

- 1. ALL CABINETS AND TERMINAL BOXES SHALL BE BONDED TO THE CONDUIT SYSTEM, AND WHERE APPLICABLE TO THE GROUND WIRE.
- 2. THE ELECTRICAL RACEWAY SYSTEM, METALLIC ELECTRICAL EQUIPMENT FRAMES, HOUSING AND ENCLOSURES SHALL BE BONDED TOGETHER AND GROUNDED.
- 3. THE EQUIPMENT BONDING JUMPERS SHALL NOT BE SMALLER THAN THE SIZES LISTED IN THE AFOREMENTIONED CODE. 4. GROUND LUGS FOR CABLE CONNECTIONS SHALL BE SIMILAR TO BURNDY, TYPE
- YAV FOR CONDUCTOR SIZES AS PERMITTED BY THE AFOREMENTIONED CODES. 5. ALL GROUNDING AND BONDING SHALL BE INSTALLED IN A NEAT AND
- WORKMANLIKE MANNER AND SHALL BE AS INCONSPICUOUS AS POSSIBLE. ALL WORK EXPOSED TO MECHANICAL DAMAGE SHALL BE PROTECTED IN AN APPROVED MANNER. ALL GROUND SCREWS AND BUSHINGS SHALL BE MADE TIGHT.
- 6. THE PROVISION OF A FULLY-WIRED GROUNDING SYSTEM DOES NOT RELIEVE THE CONTRACTOR OF HIS RESPONSIBILITY FOR PROVIDING CONTINUITY OF THE METALLIC RACEWAY SYSTEM. THE METALLIC RACEWAY SYSTEM SHALL BE ASSEMBLED AND BONDED TOGETHER TO FORM A CONTINUOUS PATH FROM THE MOST REMOTE OUTLET.
- 7. ALL GROUNDING WIRES, EXCEPT AS OTHERWISE SPECIFIED OR INDICATED ON THE DRAWINGS, SHALL BE SIZED IN ACCORDANCE WITH THE RULES OF THE AFOREMENTIONED CODE.
- 8. FOR CONNECTION TO THE GROUNDING SYSTEM, THE CONTRACTOR SHALL FURNISH AND INSTALL A GROUND LUG WELDED TO THE INTERIOR OF EVERY METALLIC BOX, CABINET, HOUSING OR ENCLOSURE WHICH IS FURNISHED UNDER THIS OR ANY OTHER SECTION OF THE SPECIFICATIONS.
- 9. EACH STEEL BOX SHALL BE CONNECTED BY THE USE OF A GROUNDING BUSHING ON RIGID CONDUIT, O.Z. TYPE BLG.
- 10. A SEPARATE GREEN INSULATED GROUND WIRE SHALL BE RUN WITH EACH CIRCUIT AS INDICATED.
- F. OUTLET BOXES:
- 1. OUTLET BOXES SHALL BE INSTALLED AT ALL LOCATIONS SHOWN ON THE DRAWINGS FOR ALL ELECTRICAL DEVICES INCLUDING CONVENIENCE RECEPTACLES AND LIGHTING FIXTURES. THE LOCATIONS OF THE OUTLETS ON THE DRAWINGS ARE APPROXIMATE. ACTUAL LOCATIONS SHALL BE COORDINATED IN THE FIELD.
- 2. ALL OUTLETS SHALL BE INSTALLED IN ACCESSIBLE LOCATIONS AND NONE SHALL BE INSTALLED ABOVE DUCTS, BEHIND FURRING OR OTHER SIMILAR LOCATIONS. ANY OUTLET DESIGNATED AS PROVIDING POWER FOR A PARTICULAR PIECE OF EQUIPMENT SHALL BE ACCESSIBLE FOR DISCONNECTION WITH SAID UNIT IN PLACE. ALL JUNCTION BOXES SHALL BE LABELED IDENTIFYING THE CIRCUIT(S) CONTAINED.
- 3. OUTLETS IN HUNG CEILING AREAS SHALL BE CONCEALED ABOVE HUNG CEILING FOR RECESSED LIGHTING FIXTURES; OR SET FLUSH WITH HUNG CEILING FOR SURFACE AND PENDANT MOUNTED LIGHTING FIXTURES. THESE OUTLETS SHALL BE SECURELY SUPPORTED FROM THE FRAMING WORK WHICH SUPPORTS THE CEILING OR FROM THE BUILDING STRUCTURE ABOVE THE CEILING.
- 4. WHERE NECESSARY FOR THE SUPPORT OF THE ELECTRICAL WORK, BARS, ANGLES OR CHANNEL MEMBERS OF SUITABLE SIZE SHALL BE FURNISHED AND INSTALLED.
- 5. MOUNTING HEIGHTS FOR ELECTRICAL DEVICES SHALL BE AS INDICATED ON ARCHITECTURAL PLANS. IF THERE ARE NO ARCHITECTURAL PLANS FOR THIS PROJECT THE MOUNTING SHALL BE AS FOLLOWS. UNLESS OTHERWISE NOTED ON THE PLANS:
- a. LIGHT SWITCHES: 48" AFF TO CENTERLINE OF BOX.
- b. WALL MOUNTED OCCUPANCY SENSORS: 48" AFF TO CENTERLINE OF BOX.
- c. RECEPTACLES: 18" AFF TO CENTERLINE OF BOX.
- d. DATA/TELEPHONE OUTLETS: 18" AFF TO CENTERLINE OF BOX.
- e. FIRE ALARM MANUAL PULL STATION: 42" MIN./48" MAX. AFF TO HANDLE. f. FIRE ALARM AUDIO AND/OR STROBE: 80" AFF TO BOTTOM OF STROBE LENS OR 6" FROM CEILING TO TOP OF STROBE LENS, WHICHEVER IS LOWER.
- 6. BLANK STEEL BOX COVERS SHALL BE INSTALLED ON ALL UNUSED OUTLETS UNLESS OTHERWISE INDICATED. IN FINISHED AREAS, BLANK COVERS SHALL BE PROVIDED. COLOR SHALL BE COORDINATED WITH THE ARCHITECT.
- 7. OUTLET BOXES FOR SWITCHES, RECEPTACLES AND COMMUNICATION OUTLETS SHALL NOT BE MOUNTED BACK-TO-BACK.

E-7 MATERIALS

A. CONDUIT:

- 7. MINIMUM SIZE OF CONDUIT SHALL BE 3/4" EXCEPT FOR LOW VOLTAGE CONTROL AND WIRING BETWEEN LIGHT FIXTURES WHERE 1/2" CONDUIT MAYBE USED OR UNLESS OTHERWISE INDICATED ON THE DRAWINGS OR SPECIFIED.
- 8. FLEXIBLE METALLIC CONDUIT, EXCEPT WHERE OTHERWISE SPECIFIED, SHALL BE SINGLE-STRIP ELECTROGAL VANIZED, SPIRALLY-WOUND, INTERLOCKED, STEEL FLEXIBLE CONDUIT.
- 9. MC CABLE MAY BE USED FOR WIRING IN CONCEALED AREAS OR AS INDICATED ON DRAWINGS. EMT SHALL BE USED IN ALL EXPOSED AREAS AND FOR WIRING PENETRATING FLOOR
- 10. LOCKNUTS SHALL BE HEAVY GAUGE SHEET STEEL TYPE WITH A PLATED CORROSION-RESISTANT COATING.
- 11. BUSHINGS SHALL BE MALLEABLE IRON INSULATED TYPE WITH A CADMIUM COATING.
- 12. ALL CONDUIT INSTALLED IN WET LOCATIONS, OR WHERE EXPOSED TO WEATHER SHALL BE RIGID GALVANIZED STEEL CONDUIT (RGS), CONDUITS INSTALLED UNDERGROUND SHALL BE SCHEDULE 40 PVC AND INTERIOR CONDUITS SHALL BE EMT U.O.N.
- 13. ALL MAIN FEEDERS AND CIRCUITRY FOR MECHANICAL EQUIPMENT OR IN EXPOSED AREAS SHALL BE IN CONDUIT.

B. SLEEVES:

SLEEVES THROUGH FIRE RESISTANT WALLS AND CEILINGS SHALL BE COMPLETELY PACKED WITH NON-COMBUSTIBLE FIRE STOP MATERIAL RATED FOR THE PARTICULAR WALL BEING PENETRATED. PENETRATIONS THRU FIRE RATED MATERIAL SHALL BE MINIMIZED.

C. WIRE AND CABLE:

1. ALL WIRE AND CABLE SHALL HAVE SOFT ANNEALED COPPER CONDUCTORS WITH 600 VOLT INSULATION, AND SHALL BE LISTED AND APPROVED BY UNDERWRITERS' LABORATORIES, AND SHALL MEET ALL SPECIFICATIONS OF THE IPCEA-NEMA STANDARDS.

2. ALL WIRE FOR GENERAL USE, UNLESS SHOWN OR SPECIFIED OTHERWISE SHALL BE TYPE THHN. ALL WIRE INSTALLED UNDERGROUND OR ON ROOFTOPS SHALL BE TYPE XHHW-2 UNLESS OTHERWISE NOTED. WIRE #10 AWG AND SMALLER SHALL BE CONSISTENTLY COLOR CODED THROUGHOUT BY MEANS OF COLORING APPLIED TO THE OUTER COVERING TO INDICATE PHASE AND NEUTRAL. ALL OTHER WIRES AND CABLES SHALL BE COLOR CODED BY APPLICATION OF A BAND OF APPROPRIATELY COLORED PLASTIC TAPE APPLIED OVER THE JACKETS AT EACH OUTLET, JUNCTION, PULL AND TERMINAL POINTS. THE COLOR CODING FOR WIRING SHALL BE:

	120/208V	277/480V
PHASE A	BLACK	BROWN
PHASE B	RED	ORANGE
PHASE C	BLUE	YELLOW
NEUTRAL	WHITE	GRAY
GROUND	GREEN	GREEN

3. ALL BRANCH CIRCUIT CONDUCTORS SHALL BE MINIMUM #12 AWG SIZE UNLESS OTHERWISE INDICATED.

4. GROUND WIRE AND CABLE SHALL BE COPPER CONDUCTORS.

5. 120 VOLT CONDUCTOR LENGTHS IN EXCESS OF 100 FEET SHALL BE #10 AWG MIN.

D. CONNECTORS FOR WIRE AND CABLE:

1. WIRE AND CABLE CONNECTORS SHALL BE SOLDERLESS, MECHANICAL, SOLID COPPER OR COPPER ALLOY TYPES. CONNECTORS SHALL BE BUCHANAN ELECTRICAL PRODUCTS COPPER SQUEEZE-ON TYPE WITH MOLDED RUBBER OR VINYL CAP, MINNESOTA MINING AND MANUFACTURING COMPANY "SCOTCHLOCK: OR IDEAL INDUSTRIES "SUPER NUT" SPRING CONNECTOR WITH MOLDED VINYL CAP.

CONNECTORS FOR CONDUCTORS LARGER THAN #8 AWG SHALL BE MECHANICAL BOLTED TYPE, INSULATED WITH CLAMP-ON MOLDED COVERS. THE MANUFACTURER SHALL BE OZ ELECTRICAL MANUFACTURING COMPANY OR BURNDY ENGINEERING COMPANY.

3. ELECTRICAL INSULATING TAPE SHALL BE VINYL PLASTIC TYPE WITH PRESSURE ADHESIVE, MINNESOTA MINING AND MANUFACTURING COMPANY "SCOTCH" NO. 33 ELECTRICAL TAPE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATION. ALL CONNECTORS AND CONNECTIONS HAVING IRREGULAR SURFACES SHALL BE PROPERLY PADDED WITH "SCOTCHFIL" PUTTY PRIOR TO APPLICATION OF TAPE.

4. ALL CABLE TIES INSTALLED IN PLENUM SHALL BE PANDUIT, TYPE HALAR, U.L. LISTED/APPROVED FOR USE IN PLENUM AREAS. ALL OTHER LOCATIONS SHALL BE NYLON TIE STRAPS AS MANUFACTURED BY THOMAS AND BETTS.

E. WIRING AND OUTLET DEVICES:

1. UNLESS OTHERWISE NOTED, WIRING DEVICES SHALL BE AS HEREIN SPECIFIED OR AS PER BUILDING STANDARDS. INDUSTRIAL GRADE. DEVICES AND COVER PLATES SHALL SHALL BE GANGED UNDER COMMON FACEPLATE U.O.N. AND SHALL MATCH EXISTING DEVICES. VERIFY IN FIELD.

2. DUPLEX RECEPTACLES SHALL BE 15 OR 20 AMPERE, TWO-POLE, THREE WIRE, 125 VOLT, SELF GROUNDING, NEMA 5-15 OR 5-20, WITH MATCHING DEVICE PLATE.

3. ISOLATED GROUND DUPLEX RECEPTACLE SHALL BE 15 OR 20 AMPERE. 125 VOLT, NEMA 5-15 OR 5-20, (ORANGE) WITH WHITE COVER PLATE.

SINGLE POLE, THREE-WAY AND FOUR-WAY SWITCHES SHALL BE 15 OR 20 AMPERE, 120/277 VOLTS, TOGGLE TYPE, WITH MATCHING DEVICE PLATE. 5. GFI RECEPTACLE SHALL BE 15 OR 20 AMPERE, TWO-POLE, THREE WIRE, 125 VOLT, NEMA 5-15 OR 5-20, WITH MATCHING DEVICE PLATE. COORDINATE COLOR

F. OUTLET AND JUNCTION BOXES:

WITH ARCHITECT.

1. RECESSED CEILING FIXTURE OUTLETS SHALL BE 4-11/16" SQUARE SHEET METAL BOX WITH BLANK COVER AND SUITABLE HANGER BAR; BOX TO BE FASTENED TO CEILING SUSPENSION MEMBERS IN AN APPROVED MANNER, NOT LESS THAN 1'-0" FROM FIXTURE OPENING.

2. EXTENSION RINGS FOR FLUSH OUTLETS SHALL BE GALVANIZED. DRAWN SHEET STEEL 4" OCTAGONAL OR SQUARE, 4-11/16" SQUARE RINGS TO SUIT FLUSH OUTLETS, 1-1/2" DEEP OR DEEPER WHERE NECESSARY.

3. ALL EQUIPMENT EXPOSED TO THE OUTDOORS SHALL BE IN A NEMA-3R ENCLOSURE, INCLUDING THE GFI RECEPTACLES.

G. MISCELLANEOUS MATERIALS:

1. PIPE STRAPS FOR EXPOSED CONDUIT SHALL BE HEAVY DUTY CADMIUM OR ZINC COATED, ONE SCREW, MALLEABLE RIGID CONDUIT CLAMPS, COMPLETE WITH BACKSTRAPS (CLAMP BACKS), APPLETON ELECTRIC COMPANY #17100 AND #27100 LINE.

2. HANGER RODS SHALL BE GALVANIZED OR CADMIUM PLATED THREADED STEEL RODS OF ADEQUATE SIZE TO SUPPORT THE LOAD WHICH THEY CARRY MINIMUM DIAMETER SHALL BE 1/2".

INSERTS IN EXISTING CONCRETE WORK SHALL BE EXPANSION ANCHORS WITH TAPPED STEEL OR BRASS CORE NUTS SET IN DRILLED HOLES. PIERCE PHILLIPS READ HEAD, STAR OR ACKERMAN-JOHNSON EXPANSION NUTS WILL BE ACCEPTABLE.

4. SPECIAL FASTENERS SHALL COMPRISE MISCELLANEOUS TYPES OF CONDUIT AND BOX FASTENERS OF MALLEABLE IRON OR STEEL WITH A CORROSION-RESISTANT COATING OF CADMIUM OR ZINC: THESE SHALL BE PROVIDED AS REQUIRED OR NECESSARY TO COMPLETE THE INSTALLATION OF ELECTRICAL WORK. THE TYPE SELECTED SHALL BE OF ADEQUATE STRENGTH FOR THE LOAD TO WHICH IT IS SUBJECTED AND OF A DESIGN SUITED TO THE INSTALLATION CONDITIONS.

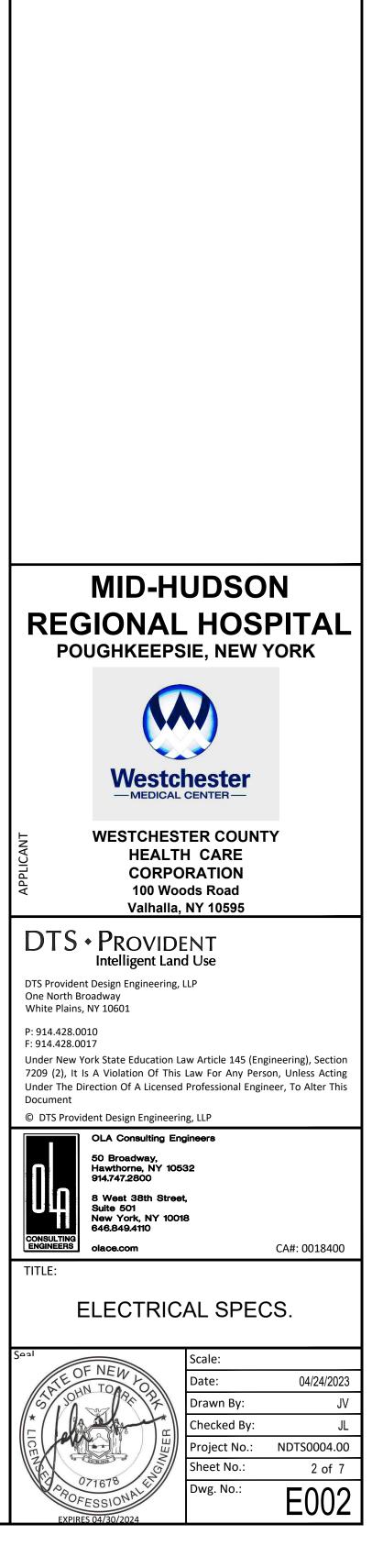
5. FASTENING HARDWARE SHALL BE CADMIUM OR ZINC-PLATED STEEL, SHEET METAL OR MACHINE SCREWS, BOLTS, NUTS, WASHERS, SHIMS AND SIMILAR FASTENING ACCESSORY HARDWARE.

REFER TO ENGINEERING DRAWINGS (ELECTRICAL, MECHANICAL OR PLUMBING) FOR DETAIL.

H. LIGHTING FIXTURES:

6

1. FURNISH AND INSTALL ALL LIGHTING FIXTURES SHOWN AND AS DESCRIBED ON THE DRAWINGS. ALL NEW FIXTURES SHALL BE AS INDICATED BY THE FIXTURE SCHEDULE. ALL FIXTURES SHALL BE FURNISHED AND INSTALLED COMPLETE WITH ALL MOUNTING HARDWARE AS REQUIRED BY SPECIFIC CEILING CONSTRUCTION OR OTHER MOUNTING METHODS. ALSO PROVIDE ALL YOKES.



NO.	REVISION	DATE
1	SITE PLAN APPLICATION	04/24/2023
2	REVISIONS PER TOWN COMMENTS	06/22/2023
3	ISSUED FOR REVIEW	08/18/2023
4	REVISED PER TOWN AND OGS COMMENTS	08/23/2023

SPECIFICATIONS CONTINUED

2. SUPPORT EACH FIXTURE SECURELY. RECESSED FLUORESCENT FIXTURES

- SHALL BE SECURED AT A MINIMUM OF TWO POINTS TO THE BUILDING STRUCTURE TO MEET THE LOCAL BUILDING CODE SEISMIC REQUIREMENTS. 3. UPON COMPLETION OF WORK AND AFTER THE BUILDING AREA IS BROOM CLEAN, ALL FIXTURES SHALL BE MADE CLEAN. USE DESTATITIZING CLOTH ON
- ALL PLASTIC AND GLASS MATERIAL 4. RELAMPING ACCESS SHALL REQUIRE NO SPECIAL TOOLS. 5. ALL FLUORESCENT FIXTURES SHALL BE EQUIPPED WITH ELECTRONIC
- BALLASTS.
- 6. ALL FLUORESCENT LAMPS SHALL BE WARM WHITE 3000K, 82 CRI UNLESS OTHERWISE INDICATED. FURNISH NEW LAMPS FOR ALL NEW FIXTURES.
- I. NEW CIRCUIT BREAKER PANELBOARDS:
- 1. FURNISH AND INSTALL CIRCUIT BREAKER PANELBOARDS AS INDICATED ON THE DRAWINGS. PANELBOARDS SHALL BE DEAD FRONT SAFETY TYPE EQUIPPED WITH THERMAL-MAGNETIC, BOLTED TYPE, MOLDED CASE CIRCUIT BREAKERS OF FRAME AND TRIP RATINGS AS SHOWN ON THE DRAWINGS. PANELBOARD BUS STRUCTURE AND MAIN LUGS OR MAIN BREAKER SHALL HAVE CURRENT RATINGS AS DRAWINGS. ALL BUSBARS SHALL BE COPPER. ALL PANELBOARDS SHALL BE SEISMIC RATED ACCORDING TO THE LOCAL BUILDING CODE REQUIREMENTS.
- 2. CIRCUIT BREAKERS SHALL BE EQUIPPED WITH INDIVIDUALLY INSULATED, UNIFORM POSITION. TRIPPED INDICATION SHALL BE CLEARLY SHOWN BY THE FOR ADDITIONAL BREAKERS SHALL BE SUCH THAT NO ADDITIONAL CONNECTORS WILL BE REQUIRED TO ADD BREAKERS.
- 3. EACH PANELBOARD, AS A COMPLETE UNIT, SHALL HAVE A RATING EQUAL TO OR GREATER THAN THE INTEGRATED EQUIPMENT RATING SHOWN ON THE DRAWINGS. PANELBOARD ASSEMBLY SHALL BE ENCLOSED IN A STEEL CABINET. THE RIGIDITY AND GAUGE OF STEEL TO BE AS SPECIFIED IN UL STANDARD 50 FOR CABINETS. THE SIZE OF WIRING GUTTERS SHALL BE IN ACCORDANCE WITH UL STANDARD 67 FOR PANELBOARDS. FRONTS SHALL FLUSH LOCK SHALL NOT PROTRUDE BEYOND THE FRONT OF THE DOOR. ALL PANELBOARD LOCKS SHALL BE KEYED ALIKE. FRONT SHALL HAVE ADJUSTABLE INDICATING TRIM CLAMPS WHICH SHALL BE COMPLETELY CONCEALED WHEN THE DOORS ARE CLOSED. DOORS SHALL BE MOUNTED BY COMPLETELY CONCEALED STEEL HINGES. FRONTS SHALL NOT BE REMOVABLE WITH DOOR IN THE LOCKED POSITION. A CIRCUIT DIRECTORY FRAME AND CARD WITH A CLEAR PLASTIC COVERING SHALL BE PROVIDED ON THE INSIDE OF THE DOOR. LONG OR EQUIVALENT FOR EACH CIRCUIT. THE DIRECTORY SHALL BE TYPED GAUGE, FULL FINISHED STEEL WITH RUST-INHIBITING PRIMER AND BAKED ENAMEL FINISH.
- 4. THE PANELBOARD INTERIOR ASSEMBLY SHALL BE DEAD FRONT WITH BARRIERED ON FIVE SIDES. THE BARRIER IN FRONT OF THE MAIN LUGS SHALL STRUCTURE OPPOSITE THE MAINS SHALL BE BARRIERED.
- 5. 208/120 VOLT PANELBOARDS SHALL BE PROVIDED WITH FACTORY INSTALLED BRAZED TO PANELBOARD ENCLOSURE.
- SHALL BEAR THE UL LABEL
- J. DISCONNECT SWITCHES:
- 1. THE CONTRACTOR SHALL FURNISH AND INSTALL FUSIBLE OR NON-FUSIBLE
- 2. THE DISCONNECT SWITCHES, UNLESS OTHERWISE INDICATED OR SPECIFIED.
- OF CABLE INDICATED ON THE DRAWINGS. 3. ALL DISCONNECT SWITCHES SHALL BE FRONT OPERATED AND EACH SHALL CONTAIN A GROUNDING LUG WELDED TO THE INSIDE OF THE SWITCH
- ENCLOSURE. 4. SWITCHES SHALL BE HORSEPOWER RATED FOR LOAD SERVED AND RATED FOR
- 200KAIC RMS SYMMETRICAL FAULT CURRENT. 5. SWITCHES SHALL HAVE PROVISIONS TO BE LOCKED IN THE OPEN POSITION
- WITH CLIPS TO ACCEPT CLASS J FUSES. 6. SWITCHES SHALL MEET NEMA STANDARD KS-1-1990 FOR TYPE HD SWITCHES

AND SHALL BE U.L. LISTED.

- N. PULLBOXES AND TROUGHS:
- 1. PULLBOXES AND TROUGHS WITH COVERS SHALL BE FABRICATED FROM WITH NICKEL OR CADMIUM PLATED, OVAL HEAD SCREWS PROVIDED WITH STOP BEAD WASHERS. TROUGHS SHALL HAVE HINGED COVERS AND SHALL BE HELD CLOSED WITH EXTERNAL CLAMPS. DIMENSIONS OF BOXES AND TROUGHS APPLICABLE CODE REQUIREMENTS.
- 2. PULLBOXES AND TROUGHS SHALL BE FINISHED INSIDE AND OUTSIDE WITH A SHOP-APPLIED COAT OF ASA #61 LIGHT GRAY ENAMEL
- 3. THE CONTRACTOR SHALL PROVIDE ALL PULLBOXES REQUIRED TO PULL WIRES IN CONDUIT RUNS WHETHER INDICATED ON THE DRAWINGS OR NOT. BOXES AND TROUGHS USING CONCENTRIC OR ACENTRIC KNOCKOUTS SHALL BE GROUNDED TO THE INCOMING CONDUITS BY MEANS OF GROUNDING FITTINGS AND BONDING JUMPERS. OZ TYPE BLG INSULATED GROUNDING BUSHINGS, AS SPECIFIED ELSEWHERE, SHALL BE USED. BONDING JUMPERS SHALL BE COPPER SIZED IN ACCORDANCE WITH THE AFOREMENTIONED CODE. A GROUND LUG SHALL BE WELDED INSIDE EACH BOX AND TROUGH.

O. ALTERATIONS AND REMOVAL OF EXISTING EQUIPMENT. CONDUIT & WIRING:

- 1. THE EXISTING BUILDING ELECTRICAL SYSTEMS SHALL BE MAINTAINED IN NOT BE SHUT DOWN NOR SHALL CONNECTIONS BE MADE THERETO WITHOUT PRIOR APPROVAL OF THE OWNER.
- 2. CERTAIN EXISTING CONDUITS AND ASSOCIATED WIRING ARE INDICATED ON THE CONTRACTOR SHALL MAKE EVERY EFFORT TO DETERMINE THE LOCATION

BACKBOXES, APPROVED HANGERS, ALL REQUIRED MISCELLANEOUS HARDWARE AND LAMPS. ALL STEEL PARTS SHALL BE BONDERIZED AND PHOSPHATIZED. ALL FIXTURES AND TRIMS SHALL BE FREE FROM LIGHT LEAKS.

BRACED AND PROTECTED CONNECTORS. THE FRONT FACES OF ALL CIRCUIT BREAKERS SHALL BE FLUSH WITH EACH OTHER. LARGE PERMANENT INDIVIDUAL CIRCUIT NUMBERS SHALL BE AFFIXED TO EACH BREAKER IN A BREAKER HANDLE TAKING A POSITION BETWEEN ON AND OFF. PROVISIONS

INCLUDE DOORS AND HAVE FLUSH, BRUSHED STAINLESS STEEL, CYLINDER TUMBLER-TYPE LOCKS WITH CATCHES AND SPRING-LOADED DOOR PULLS. THE THE DIRECTORY CARD SHALL PROVIDE A SPACE AT LEAST 1/4" HIGH AND 3" TO IDENTIFY THE LOAD FED BY EACH CIRCUIT. FRONTS SHALL BE OF CODE

PANELBOARD FRONT REMOVED. MAIN LUGS OR MAIN BREAKER SHALL BE BE HINGED TO A FIXED PART OF THE INTERIOR. THE END OF THE BUS

100% RATED NEUTRAL BUS AND GROUND BUS WHICH SHALL HAVE PROVISIONS FOR EACH CIRCUIT IN THE PANELBOARD. EQUIPMENT GROUND BUS SHALL BE

6. PANELBOARDS SHALL BE LISTED BY UNDERWRITERS' LABORATORIES AND

DISCONNECT SWITCHES AS REQUIRED AND/OR SHOWN ON THE DRAWINGS.

SHALL BE HEAVY-DUTY, QUICK-MAKE, QUICK-BREAK OPERATED, IN NEMA 1 OR 3R ENCLOSURES, OF A CAPACITY, TYPE AND NUMBER OF POLES AS NOTED ON THE DRAWINGS. THE MAIN LUGS SHALL BE ADEQUATE TO ACCEPT THE SIZES

MINIMUM #12 USSG GALVANIZED SHEET STEEL WITH ALL SEAMS AND JOINTS WELDED AND GROUND SMOOTH. COVERS SHALL BE SECURED TO PULLBOXES SHALL BE AS REQUIRED BY ARRANGEMENT OF CONDUITS, EQUIPMENT OR

OPERATION DURING THE CONSTRUCTION PERIOD. EXISTING SYSTEMS SHALL

THE DRAWINGS ACCORDING TO THE BEST INFORMATION AVAILABLE. CERTAIN OTHER EXISTING CONDUITS AND ASSOCIATED WIRING MAY NOT BE SHOWN. OF EXISTING CONDUIT AND WIRING AS REQUIRED FOR NEW CONSTRUCTION OR IF DAMAGED DURING CUTTING OPERATIONS, REPLACE/REPAIR AT NO EXPENSE TO THE OWNER.

- 3. WHERE EQUIPMENT IS REMOVED OR WHERE WALLS AND CEILINGS ARE DEMOLISHED, WIRING DEVICES, CONDUIT, WIRING AND INSTALLATION MATERIAL (FITTINGS, BOXES, HANGERS, SUPPORTS, ETC.) THAT IS NOT TO BE REUSED SHALL BE REMOVED. ALL CONDUITS REMOVED SHALL BE CUT FLUSH WITH CONSTRUCTION AND OPENINGS PATCHED. ALL WIRING REMOVED SHALL BE DISCONNECTED AS FAR BACK AS THE BRANCH CIRCUIT PANELBOARD TERMINALS UNLESS OTHERWISE NOTED. WHERE WIRING IS TO REMAIN IN EXISTING CONDUITS TO MAINTAIN CONTINUITY OF CIRCUITS AND PASSES THROUGH OUTLET BOXES NOT TO BE REUSED FOR WIRING DEVICES OR LIGHTING FIXTURES. SUCH OUTLETS SHALL BE FURNISHED WITH COVERPLATES. ACTIVE CIRCUITS, IF REQUIRED AND NECESSARY TO REMAIN, SHALL BE REROUTED WITH NEW MATERIALS.
- 4. ALL EQUIPMENT WHICH IS BEING REMOVED AND NOT BEING REUSED SHALL BE RETURNED TO THE OWNER OR DISPOSED OF AS DIRECTED.
- 5. CONTRACTOR SHALL MEASURE STEADY STATE LOAD CURRENTS ON EACH PANELBOARD FEEDER OR EACH PANELBOARD THAT WAS ALTERED. SHOULD THE DIFFERENCE AT ANY PANELBOARD BETWEEN PHASES EXCEED 20 PERCENT, REARRANGE CIRCUITS IN PANELBOARD TO BALANCE THE PHASE LOAD WITHIN 20 PERCENT. TAKE CARE TO MAINTAIN PROPER PHASING FOR MULTI-WIRE BRANCH CIRCUITS. UPDATE DIRECTORIES ACCORDINGLY.

E-9 PAINTING

A. PULL BOXES AND WIREWAYS SHALL BE SHOP PAINTED INSIDE AND OUTSIDE WITH ONE COAT OF PRIMER AND ONE COAT OF ENAMEL UNDERCOATER IN A LIGHT GRAY COLOR AS APPROVED BY THE CLIENT'S REPRESENTATIVE.

E-10 IDENTIFICATION

- A. THE CONTRACTOR SHALL PROVIDE UPDATED TYPE WRITTEN PANELBOARD DIRECTORIES IN ALL NEW PANELBOARDS AND ANY EXISTING PANELBOARD THAT HAS BEEN ALTERED. CONTRACTOR SHALL TRACE CIRCUITS TO REMAIN AS REQUIRED.
- B. ALL ELECTRICAL EQUIPMENT, SUCH AS PANELS, AND ALL OTHER SIMILAR ITEMS WHICH ARE FURNISHED UNDER THIS HEADING OF THE SPECIFICATIONS SHALL BE ADEQUATELY IDENTIFIED WITH ENGRAVED LAMINATED PLASTIC NAMEPLATE HAVING BLACK BACKGROUNDS AND WHITE LETTERS. WORDING ON THE NAMEPLATES SHALL CLEARLY INDICATE THE NAMES AND FUNCTIONS OF THE EQUIPMENT. THE CONTRACTOR SHALL SUBMIT FOR APPROVAL, FIVE COPIES OF A LIST OF ALL EQUIPMENT TO BE IDENTIFIED TOGETHER WITH THE WORDING TO BE USED ON THE NAMEPLATES BEFORE ORDERING.
- C. A MAINTENANCE LABEL SHALL BE AFFIXED TO ALL EQUIPMENT REQUIRING PREVENTATIVE MAINTENANCE. TWO COPIES OF ALL MAINTENANCE MANUALS SHALL BE PROVIDED TO THE CLIENT.
- D. ALL FEEDERS SHALL BE TAGGED WITH APPROVED-TYPE STENCILED METAL TAGS IN ALL PANELS AND PULLBOXES THROUGH WHICH THEY ARE ROUTED. THIS TAGGING SHALL INCLUDE FEEDER NUMBER, PANEL SOURCE, CIRCUIT NUMBER, FEEDER SIZE AND EQUIPMENT SUPPLIED.
- E. EACH DUPLEX AND QUAD RECEPTACLE SHALL BE LABELED WITH THE CIRCUIT NUMBER WHICH IT SERVES.

E-11 TESTING

- A. ALL CIRCUITS SHALL BE TESTED FOR UNWANTED GROUNDS AND PROPER PHASE RELATION.
- B. THE CONTRACTOR SHALL PROVIDE QUALIFIED PERSONNEL TO CONDUCT AND/OR TO ASSIST THE CLIENT'S REPRESENTATIVE TO CONDUCT OPERATING TESTS AT THE COMPLETION OF THE WORK. THESE OPERATING TESTS WILL INCLUDE CHECKING THE FOLLOWING ELECTRICAL SYSTEMS:
- 1. WIRING DEVICES: A CHECK OF RECEPTACLES SHALL BE CHECKED FOR SMOOTHNESS OF OPERATION, CLEANLINESS OF INSTALLATION, CONDUCTOR CONNECTIONS, MANUFACTURER, RATINGS AND GROUNDING CONNECTIONS. 2. LIGHTING FIXTURES: OPERATION CHECK.
- 3. LIGHTING CONTROL SYSTEMS: ENGAGE A FACTORY-AUTHORIZED SERVICE REPRESENTATIVE TO TEST AND INSPECT COMPONENTS, ASSEMBLIES, AND EQUIPMENT INSTALLATIONS, INCLUDING CONNECTIONS, PERFORM FULL OPERATIONAL TESTS. ADJUST CONTROLS AS NEEDED.
- 4. TEST ALL CIRCUITS FOR PROPER FUNCTIONING AND CONNECTION.
- 5. EMERGENCY POWER AND LIGHTING SYSTEMS VERIFICATION OF CONNECTION TO THE BUILDING EMERGENCY POWER.
- 6. ELECTRICAL CURRENT READINGS IN ALL PANELBOARDS AFFECTED BY WORK TO VERIFY BALANCING OF LOADS.
- 7. FOR ALL LOW VOLTAGE ELECTRICAL POWER CONDUCTORS AND CABLE, PERFORM EACH VISUAL AND MECHANICAL INSPECTION AND ELECTRICAL TEST STATED IN NETA ACCEPTANCE TESTING SPECIFICATION. CERTIFY COMPLIANCE WITH TEST PARAMETERS.
- 8. FOR PANELBOARDS, PERFORM EACH VISUAL AND MECHANICAL INSPECTION AND ELECTRICAL TEST STATED IN NETA ACCEPTANCE TESTING SPECIFICATION. CERTIFY COMPLIANCE WITH TEST PARAMETERS.
- 9. FOR ENCLOSED SWITCHES AND CIRCUIT BREAKERS, PERFORM EACH VISUAL AND MECHANICAL INSPECTION AND ELECTRICAL TEST STATED IN NETA ACCEPTANCE TESTING SPECIFICATION. CERTIFY COMPLIANCE WITH TEST PARAMETERS.

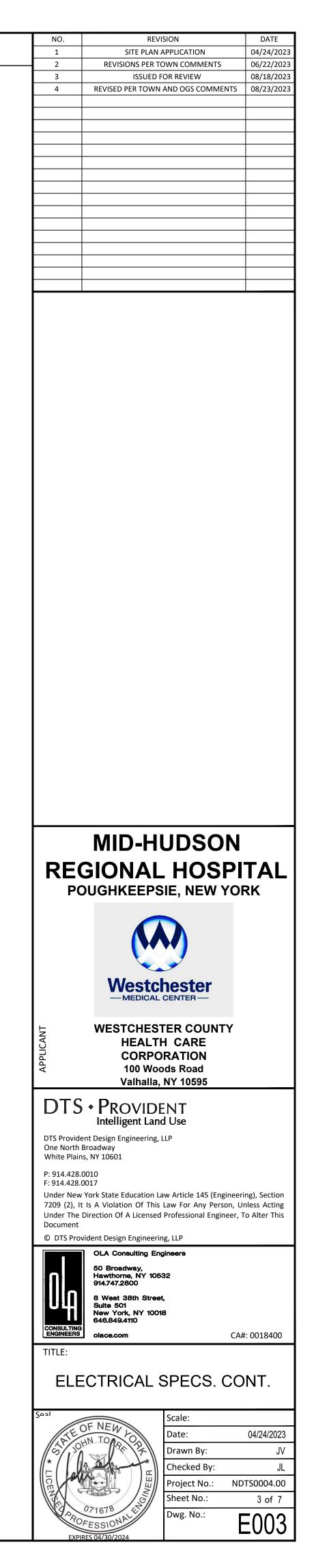
E-12 TEMPORARY LIGHT AND POWER

- A. THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND UPON COMPLETION, REMOVE SYSTEM OF TEMPORARY LIGHTING AND POWER FOR THE USE OF ALL CONSTRUCTION TRADES AS NECESSARY.
- B. WIRING SHALL BE PROVIDED FOR TEMPORARY USE DURING BUILDING CONSTRUCTION, INCLUDING GROUNDING AND FUSED MAIN CUT-OFF SWITCHES. TEMPORARY ELECTRIC LINES WITH BRANCH SWITCHES SHALL BE PROVIDED FOR LIGHTING AND FOR TAPS FOR ELECTRIC TOOLS, PUMPS AND OTHER TEMPORARY EQUIPMENT: ALL EQUIVALENT TO A MAIN LINE LOOPED THROUGH FLOOR SPACES AND UP STAIRWELLS OR SHAFTS. ALL POWER OUTLETS SHALL BE GROUNDED TO AN EQUIPMENT GROUND WIRE IN AN APPROVED MANNER. ELECTRIC LINES SHALL BE EXTENDED TO POWER TOOLS WHICH CANNOT BE LOCATED WITHIN REACH OF EXTENSION CORDS.
- C. LIGHT BULBS SHALL BE PROVIDED IN SUFFICIENT QUANTITY TO LIGHT THE BUILDING FOR SAFETY PURPOSES. EXTENSION CORDS SHALL BE PROVIDED AS MAY BE ESSENTIAL TO THE PROPER EXECUTION OF THE WORK.

D. TEMPORARY LIGHTING SHALL BE PROVIDED FOR ALL STAIRS AND OTHER LOCATIONS WHERE NEEDED FOR SAFETY OR THE PROPER EXECUTION OF WORK AND SHALL CONFORM TO ALL OSHA STANDARDS.

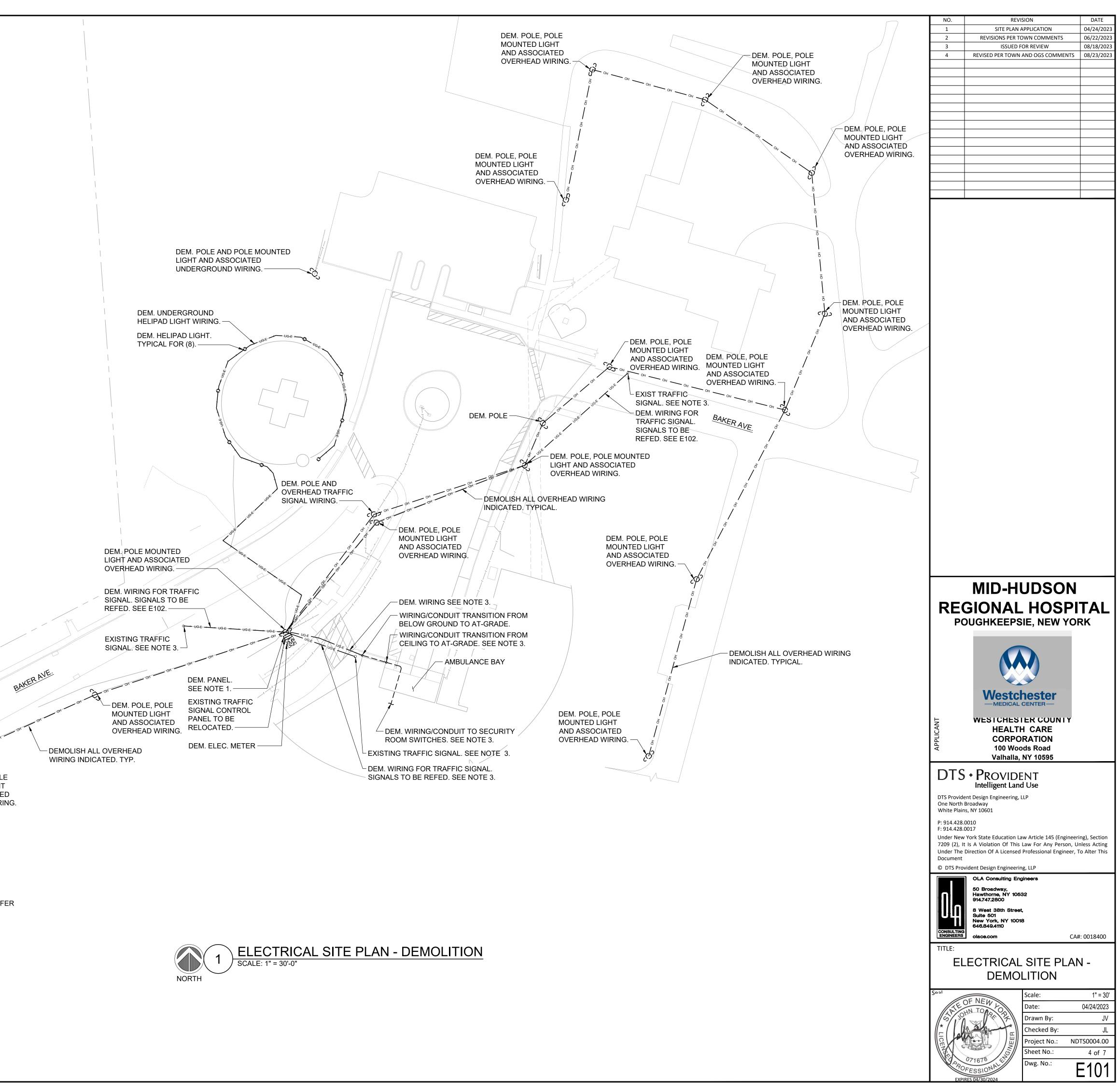
E. THE ELECTRICAL CONTRACTOR SHALL MAINTAIN TEMPORARY LIGHTING AND POWER SYSTEMS IN GOOD WORKING CONDITION, INCLUDING THE RELOCATION AND REINSTALLATION WHEN REQUIRED TO AVOID INTERFERENCE WITH THE PROGRESS OF CONSTRUCTION.

F. PROVIDE GROUND FAULT PERSONNEL PROTECTION FOR ALL SINGLE PHASE, 15 AND 20 AMPERE RECEPTACLES. ALL RECEPTACLES AND PORTABLE CORD CONNECTORS SHALL HAVE NEMA STANDARD LOCKING TYPE CONFIGURATIONS AND SHALL CONFORM TO ALL OSHA STANDARDS.

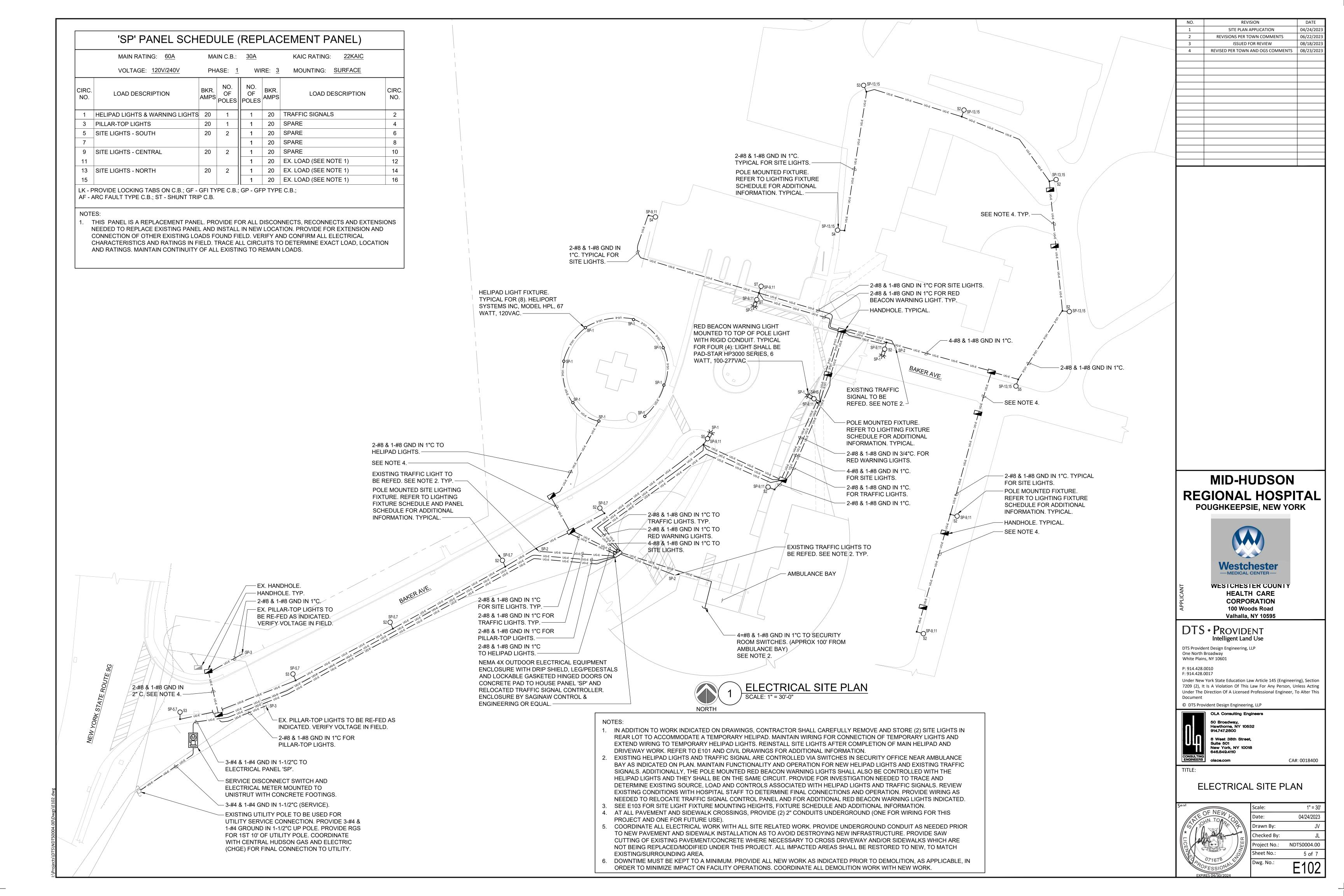


	GENERAL NOTES	
1.	PRIOR TO DEMOLITION, CONTRACTOR SHALL TRACE ALL CIRCUITS AND WIRING, AND VERIFY SOURCE AND LOAD. MAINTAIN CONTINUITY OF SERVICE TO ALL EXISTING LOADS TO REMAIN, WHETHER INDICATED ON PLANS OR NOT. MODIFY/EXTEND EXISTING WIRING AS NEEDED TO ACCOMMODATE WORK ASSOCIATED WITH THIS PROJECT AND MAINTAIN SERVICE TO ALL CIRCUITS AND LOADS.	
2.	COORDINATE ALL WORK ASSOCIATED WITH POLES AND OVERHEAD WIRING WITH CENTRAL HUDSON GAS & ELECTRIC CORP. CONTACT BRIAN FINN AT BFINN@CENHUD.COM.	
3.	EXISTING TRAFFIC SIGNALS TO REMAIN. ASSOCIATED CONTROLLER TO BE RELOCATED AS INDICATED. WIRING TO BE DEMOLISHED/REPLACED AS INDICATED ON PLAN, BACK TO SECURITY ROOM SWITCH. CONTRACTOR SHALL INCLUDE ALL WORK REQUIRED TO REFEED SIGNALS, INCLUDING DISCONNECTION OF EXISTING WIRING, AND RECONNECTION OF NEW WIRING AT TRAFFIC SIGNAL LIGHTS AS INDICATED. REFER TO E102 FOR ADDITIONAL INFORMATION. COORDINATE WITH HOSPITAL STAFF AND ASSOCIATED VENDOR(S) AS NEEDED.	
4.	HELIPAD LIGHTS AND ASSOCIATED WIRING TO BE DEMOLISHED BACK TO PANEL, AND BACK TO SECURITY ROOM SWITCH, AS NOTED ON PLAN. NEW HELIPAD LIGHTS, RED BEACON WARNING LIGHTS AND WIRING SHALL BE PROVIDED AS INDICATED ON E102.	
5.	FUNCTIONALITY AND OPERATION OF ALL LIGHTS, SIGNALS AND WARNING BEACONS ASSOCIATED WITH HELIPAD AND APPROACH/TAKEOFF SHALL BE MAINTAINED UNLESS OTHERWISE NOTED. REFER TO DRAWING E102 FOR ADDITIONAL INFORMATION.	
6.	IN ADDITION TO WORK INDICATED ON DRAWINGS, CONTRACTOR SHALL CAREFULLY REMOVE AND STORE (2) SITE LIGHTS IN REAR LOT TO ACCOMMODATE A TEMPORARY HELIPAD. MAINTAIN WIRING FOR CONNECTION OF TEMPORARY LIGHTS. REINSTALL SITE LIGHTS AFTER COMPLETION OF MAIN HELIPAD AND DRIVEWAY WORK. REFER TO E102 AND CIVIL DRAWINGS FOR ADDITIONAL INFORMATION.	
	DEMOLITION NOTES]
1.	ALL EQUIPMENT SHALL BE DISCONNECTED AND REMOVED BACK TO POWER SOURCE ORIGINATION UNLESS OTHERWISE NOTED (UON) EXISTING TO REMAIN (EX.).	
2.	CONTRACTOR SHALL VERIFY EXTENT OF DEMOLITION WORK IN THE FIELD PRIOR TO BID AND SHALL INCLUDE ALL LABOR AND MATERIALS IN BASE BID INCLUDING ALL TEMPORARY CONNECTIONS, CONDUIT AND WIRE IN ORDER TO ACCOMMODATE CONSTRUCTION AND PROVIDE CONTINUOUS SERVICE TO DEVICES AND SYSTEMS TO REMAIN, TEMPORARY AND PERMANENTLY. WORK REQUIRING THE SHUT-DOWN OF THE BUILDING POWER SHALL BE PERFORMED DURING OVERTIME AND SHALL BE INCLUDED IN BASE BID.	
3.	CIRCUIT BREAKER, CONDUIT AND CONDUCTOR SIZES INDICATED SHALL BE FIELD VERIFIED PRIOR TO BID.	
4.	ALL EXISTING ELECTRICAL EQUIPMENT NO LONGER IN USE, SUCH AS DISCONNECT SWITCHES, MOTOR CONTROLLERS, MOTOR STARTER PANELS, ETC. SHALL BE REMOVED UON.	
5.	ALL DISCONNECTED & REMOVED EXISTING ELECTRICAL ITEMS THAT ARE NOT BEING REUSED SHALL BE RETURNED TO THE OWNER OR DISPOSED OF AS DIRECTED.	
6.	THE CONTRACTOR SHALL INCLUDE IN THE BASE BID FOR ALL MATERIAL & LABOR REQUIRED FOR THE EXTENSIONS, REROUTING & RELOCATION OF EXISTING SYSTEM COMPONENTS, EQUIPMENT, WIRING, CONDUITS & CABLING SO AS TO MAINTAIN OPERATION OF ALL SYSTEMS THROUGHOUT THE BUILDING DURING DEMOLITION & CONSTRUCTION PHASES.	
		UND WIRE/CONDUIT.
	REFER TO E RE-FEED AS PILLAR-TOP	
		E RE-FED,
	EX. HANDHOLE.	. INFORMATION.
	EX. UNDERGROUND MOUNTED L CONDUIT/WIRING TO BE AND ASSOC	IGHT IATED
	DURING CONSTRUCTION. TYP DEM. UNDERGROUND	WIRING.
	WIRE/CONDUIT. REFER TO E-102 FOR WORK TO	OH OH
	RE-FEED ASSOCIATED PILLAR-TOP LIGHTS	DEM. POLE, POL MOUNTED LIGHT AND ASSOCIATE OVERHEAD WIR
	Contraction of the second seco	IG PILLAR-TOP O BE RE-FED. TO E-102 FOR DNAL INFORMATION.
	DEM. UNDERGROUND WIRE	-FEED
•••	ASSOCIATED PILLAR-TOP L	WITCH, WIRE TROUGH, AND
· · · · · · · · · · · · · · · · · · ·	DEMOLISH DISCONNECT SV WOODEN STANCHION. SER	

EXISTING POLE TO REMAIN AND BE USED FOR NEW ELECTRICAL SERVICE. SEE E102 FOR ADDITIONAL INFO.







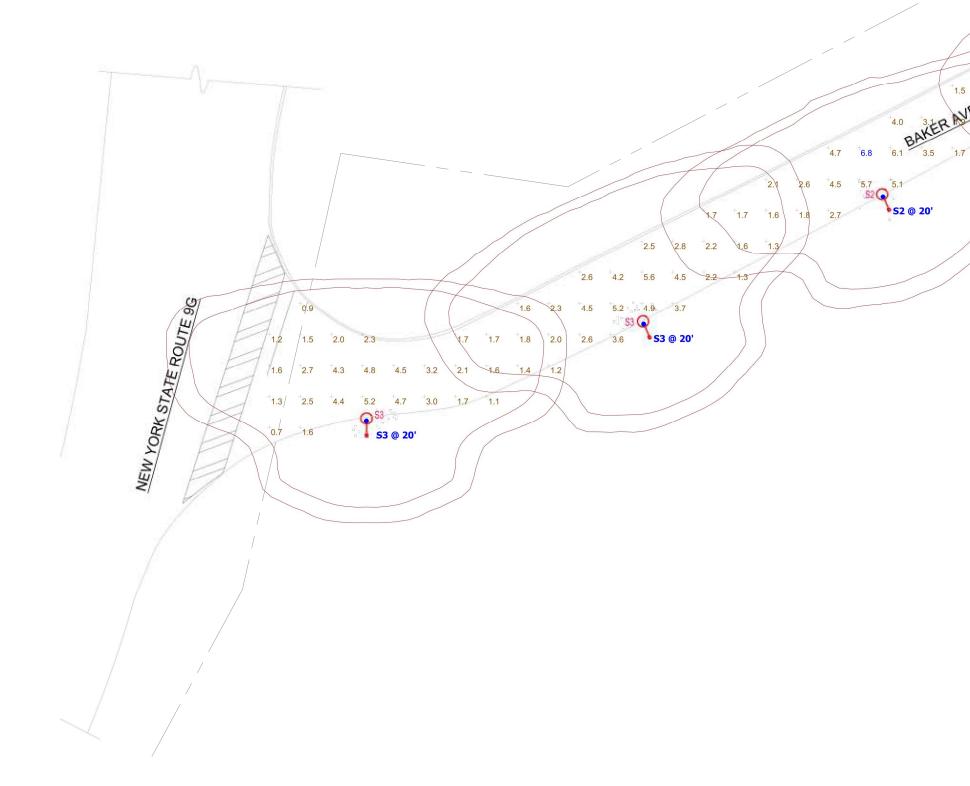
Schedul	e										
Symbol	Label	Voltage	QTY	Manufacturer	Catalog	Description	Number Lamps	Lamp Output	LLF	Input Power	Polar
•	S 2	120-277VAC	10	Cyclone Lighting	TRIS2A-FGC-T2M-P80- 30K	Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T2M Distribution.	1	14139	0.85	126.81	Max: 8250
0	S 3	120-277VAC	3	Cyclone Lighting	TRIS2A-FGC-T3M-P80- 30K	Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T3M Distribution.	1	14452	0.85	126.7	Max: 8866
0	S4H 120-277VAC 1 Cyclone Lighting TRIS2A-FGC-T4HS-P80- 30K		Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T4HS Distribution.	1	1 11136		129.7	Max: 8064			
0	S5	120-277VAC	2	Cyclone Lighting	TRIS2A-FGC-T5-P80-30K	Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T5 Distribution.	1	14000	0.85	126.97	<u>ح</u> Max: 5240
0.0	ST	120-277VAC	1	Cyclone Lighting	TRIT4A-FGC-T3M-P30- 30K	Trika TRIT4A, Pole Mounted, Cast A356 Aluminum Housing, Round Shape, Class 2 Driver Module, Black, Textured Finish, Type T3M Distribution.	1	4631	0.85	87.6	
0	S 4	120-277VAC	2	Cyclone Lighting	TRIS2A-FGC-T4-P80-30K	Trika TRIS2A, Pole Mounted Cast A356 Aluminum Housing, Class 2 Driver Module, Black, Textured Finish, Type T4 Distribution.	1	13294	0.85	127.02	Max: 7699

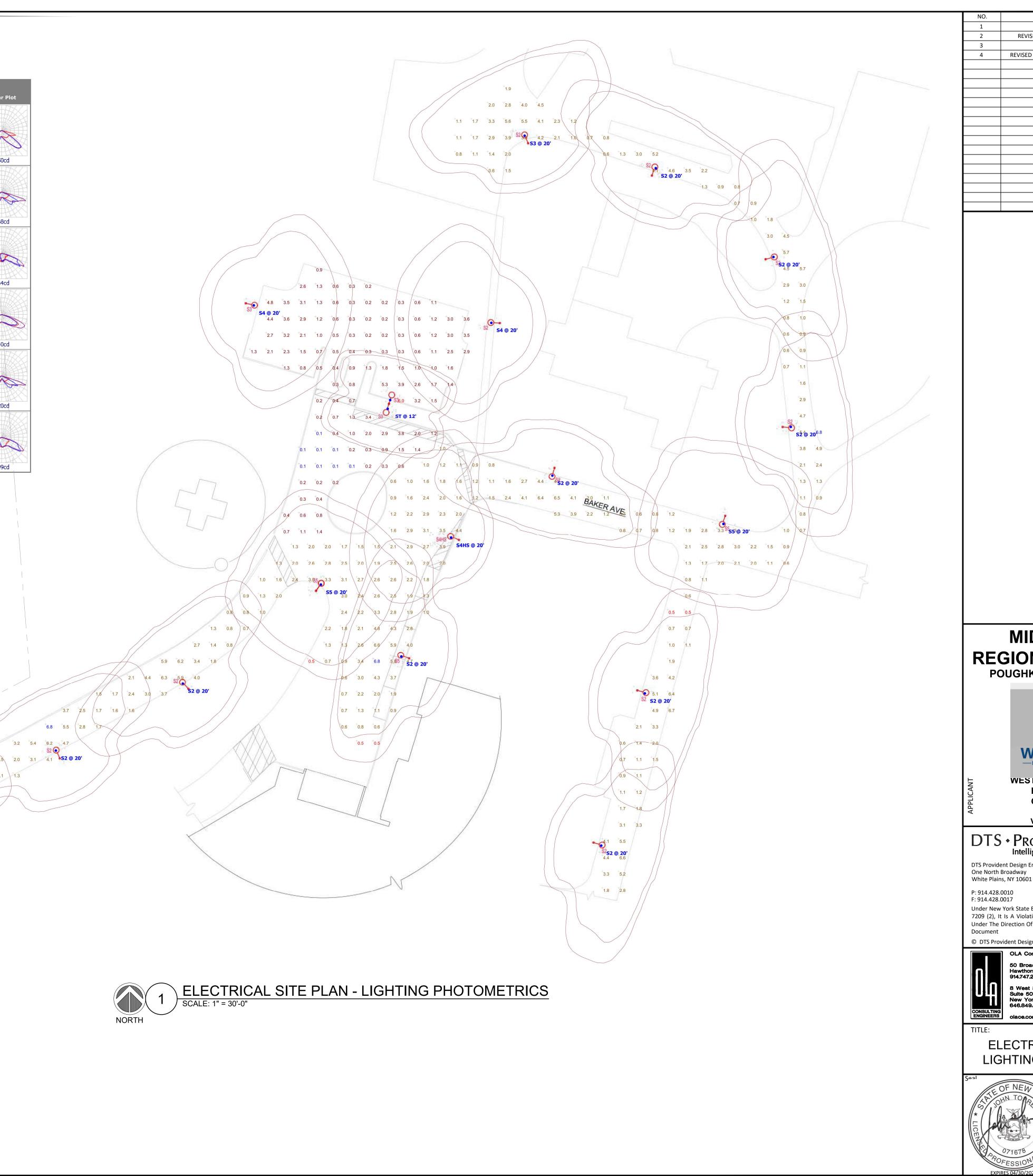
Statistics						
Description	Symbol	Мах	Avg	Min	Max/Min	Avg/Min
Roadway	+	6.8 fc	2.4 fc	0.5 fc	13.6:1	4.8:1
Parking 1 / Helipad approach	+	6.0 fc	1.3 fc	0.1 fc	60.0:1	13.0:1

NOTES:

1. POLE HEIGHTS INDICATED ON PLAN ARE TOTAL HEIGHT, TO TOP OF FIXTURE. ACTUAL POLES SHOULD BE 2' SHORTER THAN HEIGHT INDICATED, TO ACCOUNT FOR 2' CONCRETE BASE. TOTAL HEIGHT SHALL NOT EXCEED THAT

INDICATED ON PLAN. 2. FIXTURES SHALL BE CONTROLLED VIA INTEGRAL PHOTOCELL AND SHALL TURN ON AT DUSK AND OFF AT DAWN.

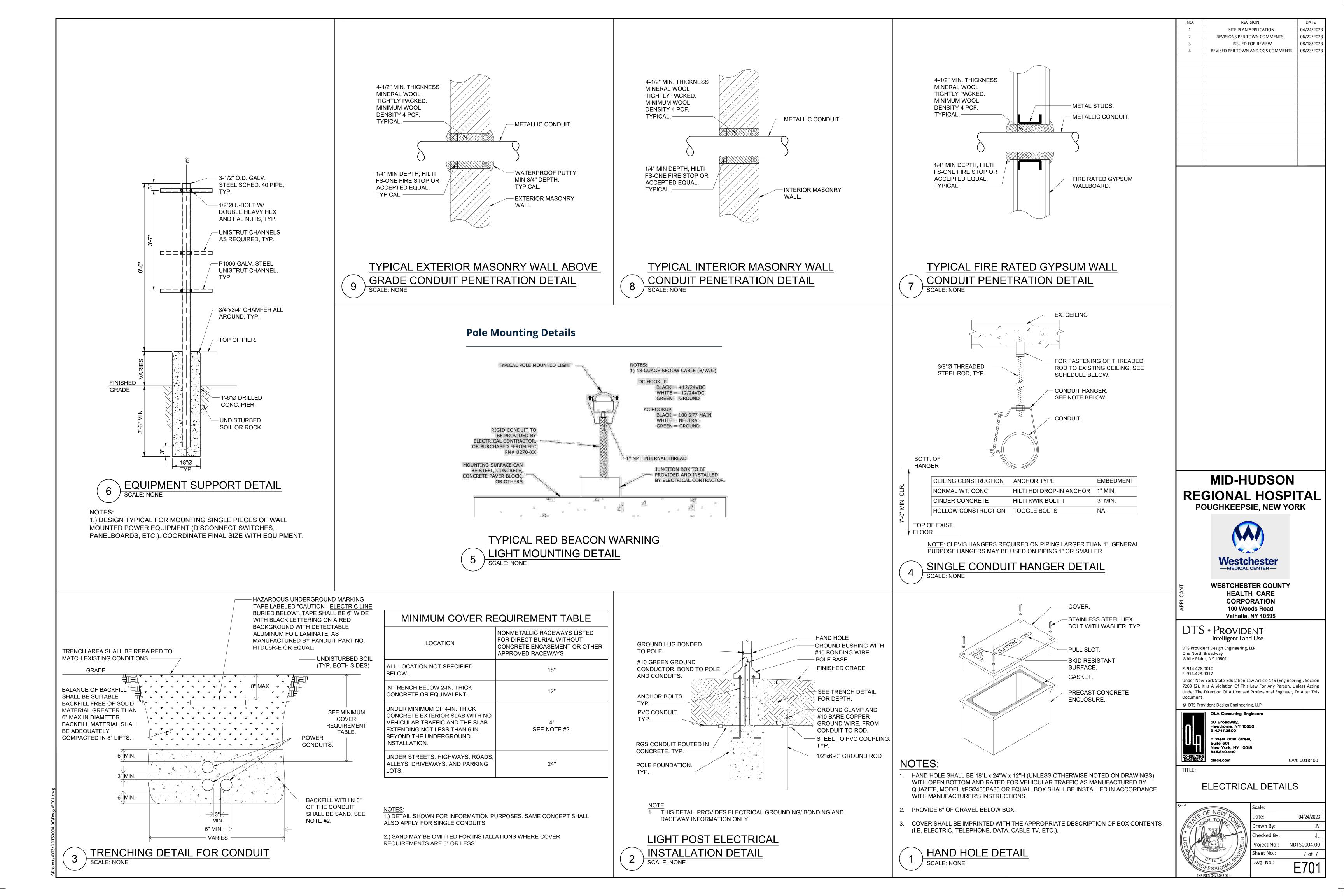


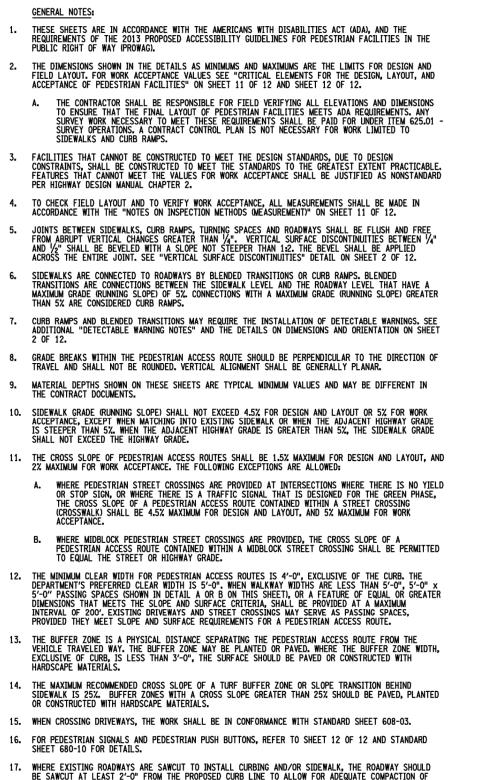


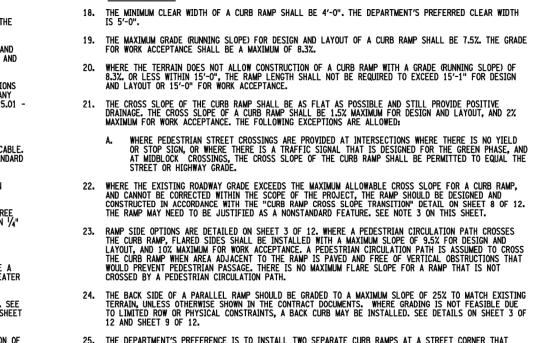
NO. 1 2 3	REVISION	DATE
2 3	SITE PLAN APPLICATION	DATE 04/24/2023
	REVISIONS PER TOWN COMMENTS	06/22/2023
4	ISSUED FOR REVIEW REVISED PER TOWN AND OGS COMMENTS	08/18/2023 08/23/2023
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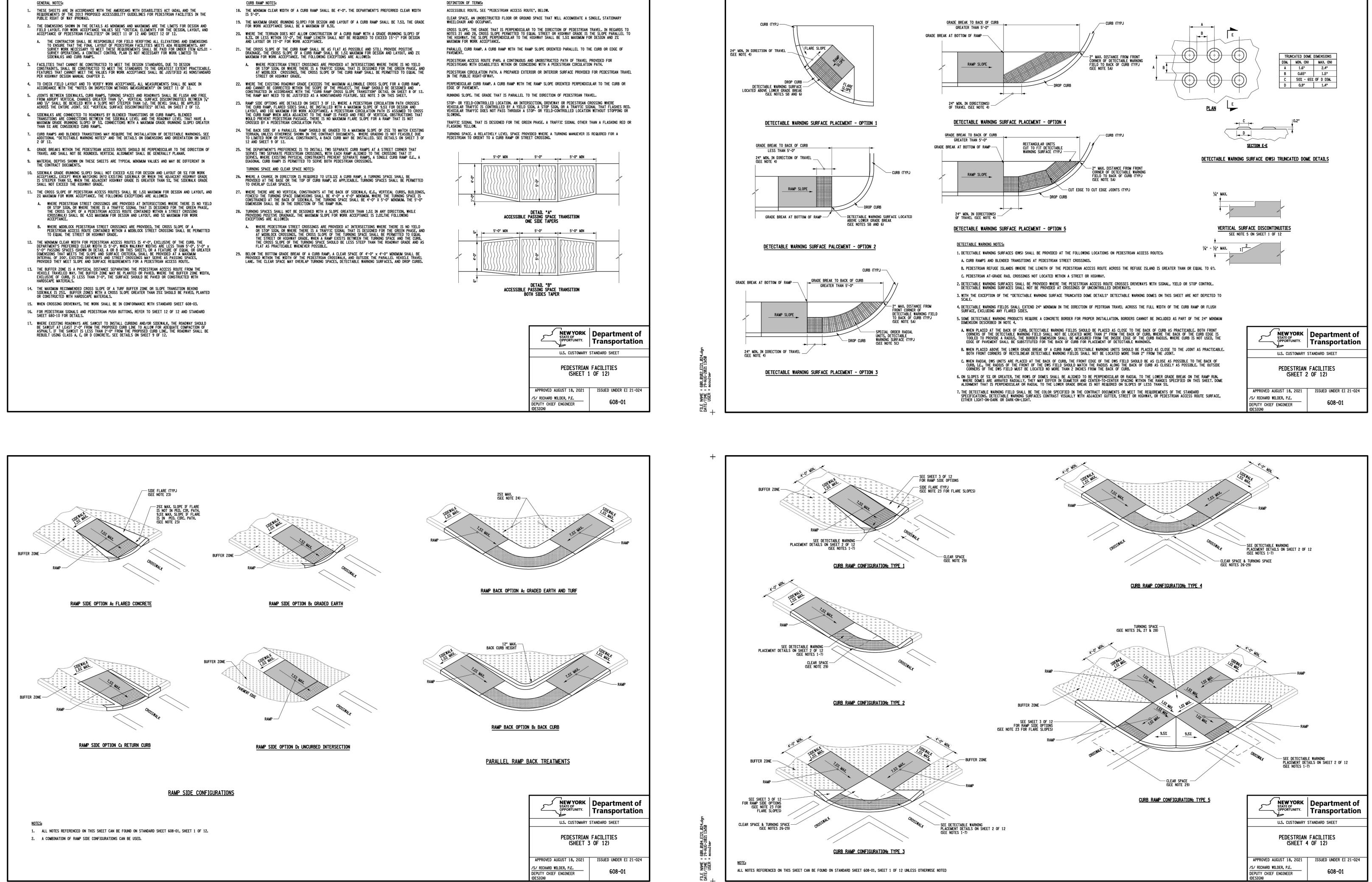






- TURNING SPACE AND CLEAR SPACE NOTES:

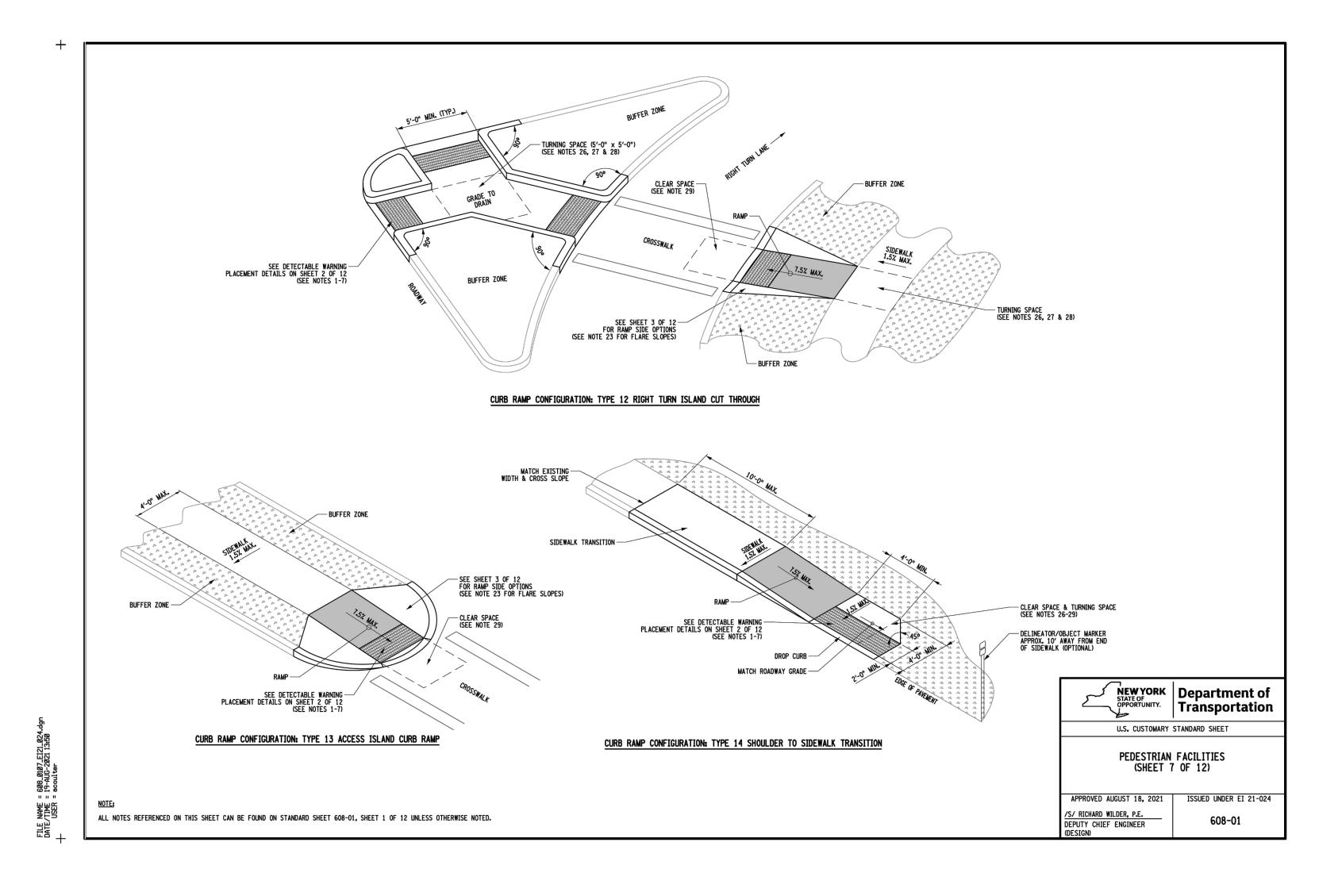
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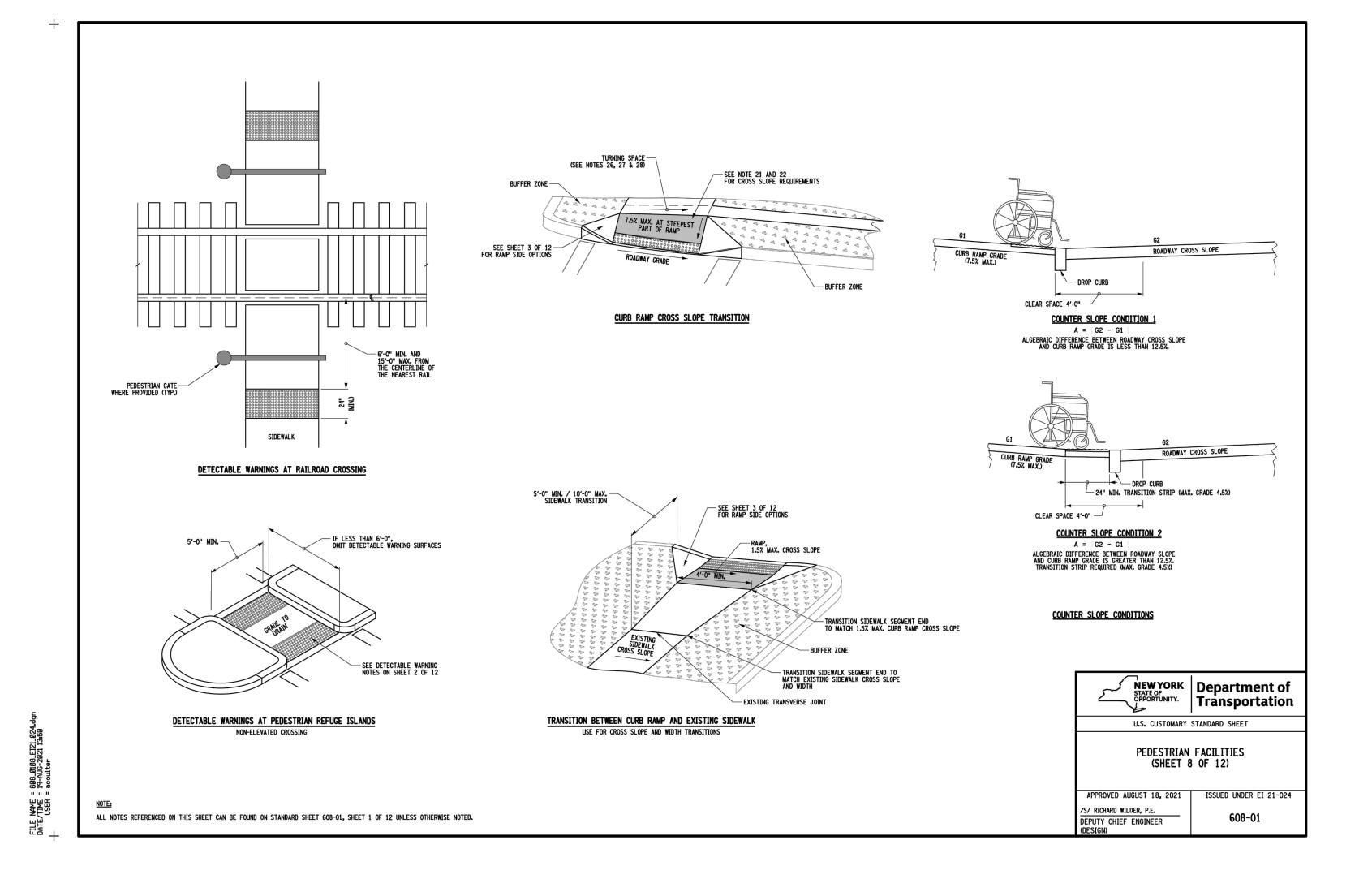
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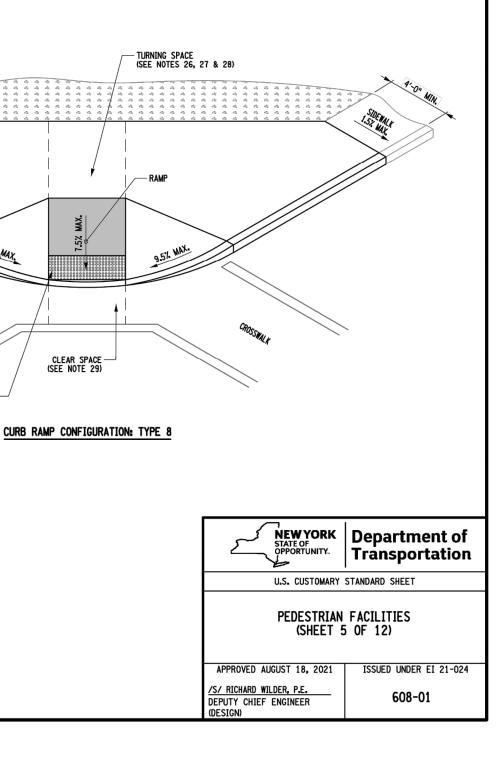
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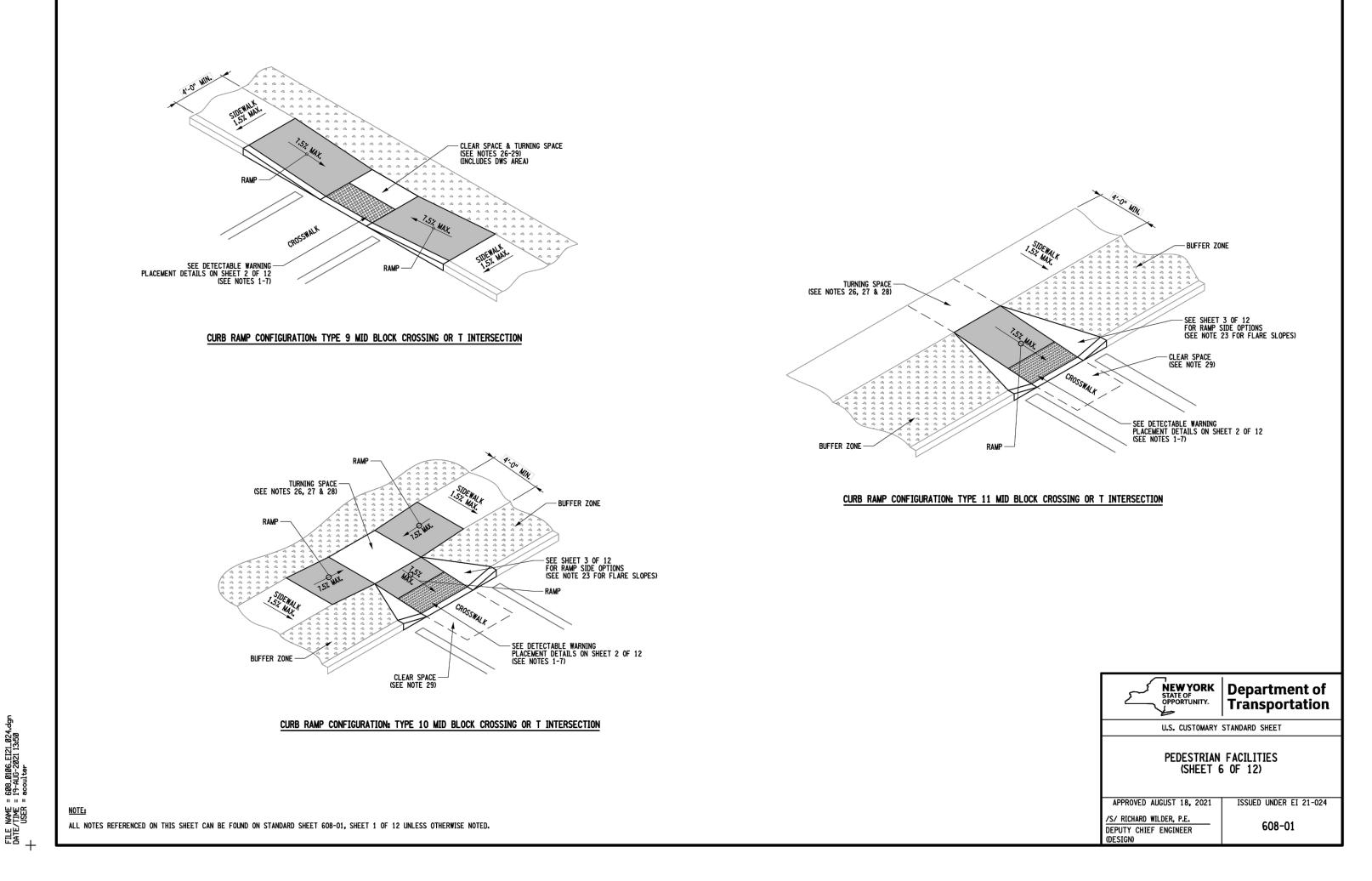
- CLEAR SPACE & TURNING SPACE (SEE NOTES 26-29) (INCLUDES DWS AREA) SEE DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 2 OF 12 (SEE NOTES 1-7) SIDEWALAN. CURB RAMP CONFIGURATION: TYPE 6 9.5% MAX. — TURNING SPACE (TYP.) (SEE NOTES 26, 27 & 28) SIDE FLARE (TYP.) — (SEE NOTE 23 FOR FLARE SLPOES) BUILDING OR OTHER STRUCTURE BUFFER ZONE -BUFFER ZONE SEE DETECTABLE WARNING-PLACEMENT DETAILS ON SHEET 2 OF 12 (SEE NOTES 1-7) SEE SHEET 3 OF 12-FOR RAMP SIDE OPTIONS (SEE NOTE 23 FOR FLARE SLOPES) -SEE DETECTABLE WARNING PLACEMENT DETAILS ON SHEET 2 OF 12 (SEE NOTES 1-7) FILE NAME = 608.0105_EI21.024.c DATE/TIME = 19-AUG-2021 13:50 + USER = acoulter — SIDE FLARE (TYP.) — (SEE NOTE 23 FOR FLARE SLOPES) - CLEAR SPACE (SEE NOTE 29) CURB RAMP CONFIGURATION: TYPE 7 NOTE: ALL NOTES REFERENCED ON THIS SHEET CAN BE FOUND ON STANDARD SHEET 608-01, SHEET 1 OF 12 UNLESS OTHERWISE NOTED.

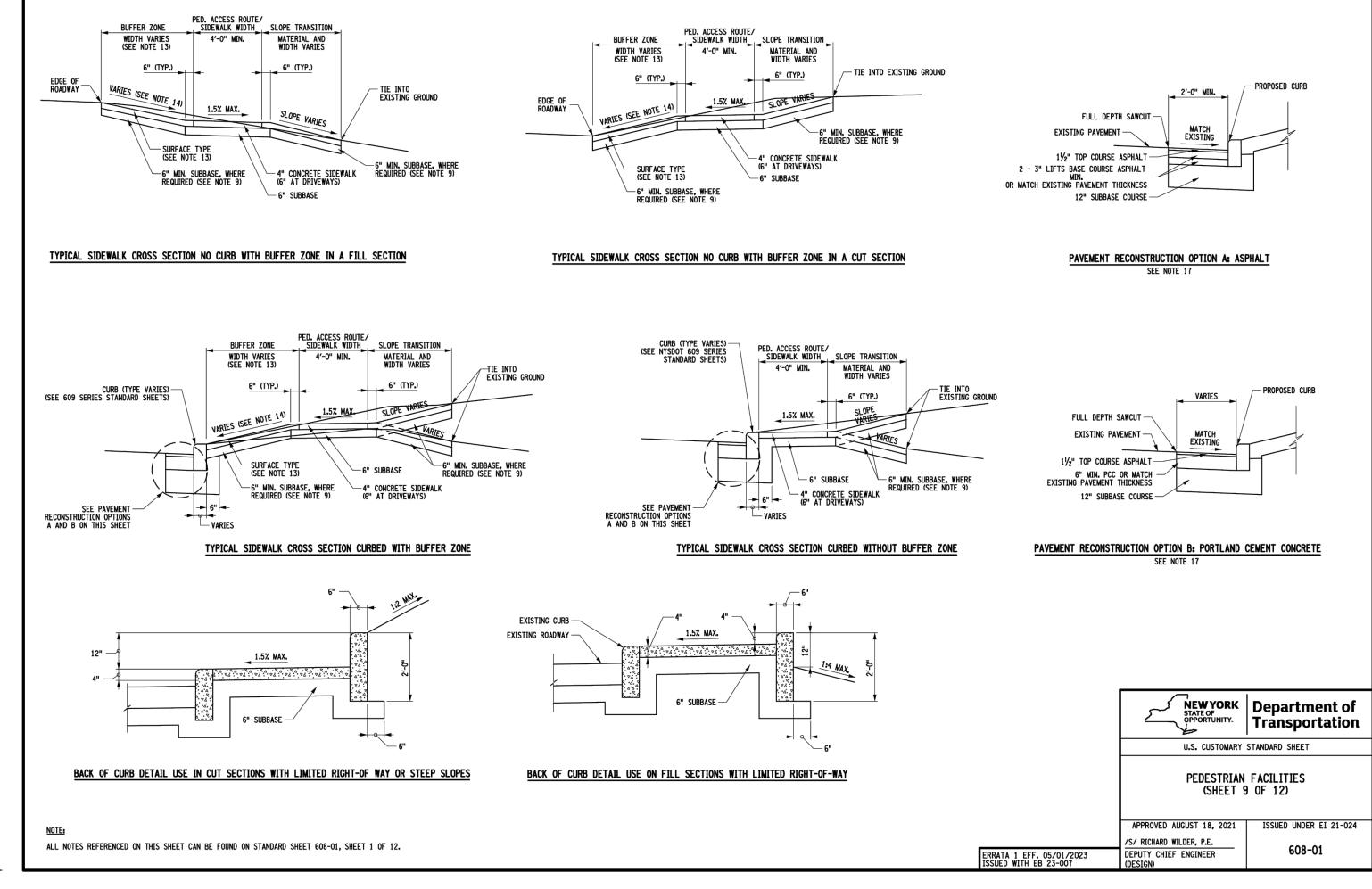


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- GENERAL NOTES FOR PEDESTRIAN FACILITIES
- PEDESTRIAN FACILITIES MUST MEET THE VALUES SHOWN ON THE APPLICABLE TABLE, OR BE JUSTIFIED AS NONSTANDARD FEATURES USING EXHIBIT 2-15A FROM THE NYSDOT HIGHWAY DESIGN MANUAL, CHAPTER 2. CURB RAMPS TO REMAIN ON 1R PROJECTS:
- 2. ON 1R PROJECTS, CURB RAMPS BUILT PRIOR TO MARCH 15, 2012 DO NOT NEED TO BE REPLACED OR MODIFIED TO MEET CURRENT (PROWAG) STANDARDS IF THEY COMPLY WITH THE CURB RAMP REQUIREMENTS IN THE 1991 ADAAG STANDARDS, SHOWN ON TABLE 1.
- 3. THE MAX. CROSS SLOPE FOR ANY PART OF AN ACCESSIBLE ROUTE IN THE 1991 ADAAG STANDARDS IS 2%. HOWEVER, CURB RAMP CROSS SLOPES AT INTERSECTIONS WITHOUT YIELD OR STOP CONTROL CAN MATCH THE HIGHWAY GRADE.
- 4. AS DETAILED IN THE "SUPPLEMENT TO THE 2013 DOJ/DOT JOINT TECHNICAL ASSISTANCE ON THE TITLE II OF THE ADA REQUIREMENTS TO PROVIDE CURB RAMPS WHEN STREETS, ROADS, OR HIGHWAYS ARE ALTERED THROUGH RESURFACING", DETECTABLE WARNINGS ARE REQUIRED ON CURB RAMPS BUILT OR ALTERED DURING THE FOLLOWING PERIODS:
- JULY 26, 1991 MAY 12, 1994 JULY 26, 1998 DECEMBER 23, 1998 JULY 26, 2001 MARCH 15, 2012 IF A CURB RAMP WAS BUILT OR LAST ALTERED DURING THE PERIODS ABOVE, OR IF THE DATE OF CONSTRUCTION OR LAST ALTERATION CANNOT BE DETERMINED, THE RAMP REQUIRES A DETECTABLE WARNING.
- 5. REQUIREMENTS FOR DETECTABLE WARNING DIMENSIONS AND PLACEMENT ARE TO BE EVALUATED BY CURRENT (PROWAG) STANDARDS AS DETAILED ON SHEET 2 OF 12 FOR CROSSWALK REQUIREMENTS ON 1R PROJECTS, REFER TO VALUES ON TABLE 2, SHEET 12 OF 12, FOR NEW AND REPLACEMENT FACILITIES.
- NOTES ON NEW AND REPLACEMENT PEDESTRIAN FACILITIES:
- 7. IF A SIDEWALK OR OTHER PEDESTRIAN PATH IS WIDER THAN 48 IN., ONLY THE MINIMUM CONTINUOUS ACCESSIBLE CLEAR WIDTH OF 48 IN. (WITH PASSING SPACES AS NEEDED) IS REQUIRED TO MEET THE SURFACE AND SLOPE REQUIREMENTS SHOWN ON TABLE 2, SHEET 12 OF 12. 8. REQUIREMENTS FOR HEIGHT AND OPENINGS ON GUARDS DO NOT APPLY TO HIGHWAY SEGMENTS, HIGHWAY RAMPS, OR HIGHWAY BRIDGES WITH BRIDGE OR HIGHWAY BARRIERS.
- NOTES ON INSPECTION METHODS (MEASUREMENT):
- 9. GRADE (RUNNING SLOPE) IS TO BE MEASURED AS AN AVERAGE OF AT LEAST 3 DIGITAL LEVEL MEASUREMENTS USING A 4-FOOT STRAIGHT EDGE, ALONG THE CENTERLINE (DIRECTION OF TRAVEL), AND OFFSET AT LEAST 18" TO EACH SIDE OF THE CENTERLINE OF THE RAMP, ROADWAY, CURB RAMP, ETC. ACCEPTANCE IS BASED ON GRADES LESS THAN OR EQUAL TO THE LIMIT SHOWN IN THE TABLE.
- 10. CROSS SLOPE IS TO BE MEASURED AS AN AVERAGE OF AT LEAST 2 DIGITAL LEVEL MEASUREMENTS, MADE PERPENDICULAR TO THE CENTERLINE (DIRECTION OF TRAVEL) OF THE SIDEWALK, TAKEN (WHEN LENGTH ALLOWS) AT 5' TO 10' INTERVALS. FOR FLARES, USE AN AVERAGE OF AT LEAST 2 DIGITAL LEVEL MEASUREMENTS TAKEN WITHIN THE PEDESTRIAN CIRCULATION PATH AND PARALLEL TO THE CURB LINE. ACCEPTANCE IS BASED ON SLOPES LESS THAN OR EQUAL TO THE LIMIT SHOWN IN THE TABLE.
- 11. AREA SLOPE IS TO BE MEASURED BY TAKING GRADE (RUNNING SLOPE) AND CROSS SLOPE MEASUREMENTS, AS DESCRIBED IN NOTES 9 AND 10. ACCEPTANCE IS BASED ON SLOPES LESS THAN OR EQUAL TO THE LIMIT SHOWN IN THE TABLE.
- WIDTHS AND OFFSETS ARE TO BE MEASURED AS AN AVERAGE OF AT LEAST TWO (2) TAPE MEASUREMENTS TAKEN PERPENDICULAR TO THE CENTERLINE (DIRECTION OF TRAVEL) OF THE PEDESTRIAN ROUTE OR PATH, AT EITHER THE RESTRICTION POINT OR (WHEN LENGTH ALLOWS) 10' INTERVALS. ACCEPTANCE IS BASED ON WIDTHS GREATER TO OR EQUAL TO THE LIMIT SHOWN IN THE TABLE.
- 13. LENGTH IS TO BE MEASURED AS AN AVERAGE OF AT LEAST TWO (2) TAPE, WHEEL, OR REEL MEASUREMENTS TAKEN WITHIN A FOOT OF THE CENTERLINE (DIRECTION OF TRAVEL) OR RAILING, RAMP, SIDEWALK, ETC., WITH THE MEASUREMENT ALONG THE GRADE OF THE RAILING, SIDEWALK, RAMP, CURB RAMP, ETC. (I.E., NOT A HORIZONTAL MEASUREMENT FOR SLOPED SURFACES). ACCEPTANCE IS BASED ON LENGTHS GREATER THAN OR EQUAL TO THE LIMIT SHOWN IN THE TABLE.
- 14. RISE OR VERTICAL DISTANCE IS TO BE MEASURED AS AN AVERAGE OF AT LEAST TWO (2) VERTICAL MEASUREMENTS. FOR LANDINGS OR STEP RISERS, TAKE MEASUREMENTS AT LEAST ONE (1) FOOT APART. FOR LANDINGS, AVERAGE AT LEAST TWO (2) MEASUREMENTS USING A SURVEY ROD AND LEVEL AT THE NOSE/EDGE OF THE LANDING AND THE BASE OF RAMP OR STAIRS. ACCEPTANCE IS BASED ON RISES OR VERTICAL DISTANCES LESS THAN OR EQUAL TO THE LIMIT SHOWN IN THE TABLE.
- 15. OPENING LIMITATIONS ARE ACCEPTABLE IF THE OPENING CANNOT PASS A SPHERE OF THE GIVEN DIAMETER. ELONGATED OPENINGS ON WALL SURFACES, SHALL BE ORIENTED WITH LONG DIMENSION PERPENDICULAR TO DIRECTION OF PEDESTRIAN TRAVEL.
- 16. DIAMETER, PERIMETER, AND ALL OTHER MEASUREMENTS ARE TO BE MEASURED AS DIRECTED BY ITEM SPECIFICATION.
- 17. PEDESTRIAN PUSHBUTTON HEIGHT IS TO BE MEASURED AS A SINGLE MEASUREMENT FROM THE CENTER OF THE PUSHBUTTON TO THE POINT DIRECTLY BELOW IT ON THE CLEAR SPACE. ACCEPTANCE IS BASED ON A MEASUREMENT WITHIN THE ACCEPTABLE RANGE OF HEIGHTS SHOWN IN TABLE 2.

TABLE 1 - CRITICAL ELEMENTS FOR THE DESIGN, LAYOUT AND ACCEPTANCE OF PEDESTRIAN FACILITIES. EXISTING CURB RAMPS TO REMAIN ON 1R PROJECTS (1991 ADAAG STANDARDS)

ELEMENT	LIMITS REQUIRED TO RETAIN EXISTING CURB RAMP (SEE NOTE 1)	INSPECTION METHODS (NOTE NO.)
CURB RAMPS		
CLEAR WIDTH	36" MIN.	12
FLARE SLOPE FOR RAMP WITHIN A PEDESTIAN CIRCULATION PATH	10% MAX.	10
FLARE SLOPE/EDGE FOR RAMP OUTSIDE A PEDESTRIAN CIRCULATION PATH	NO MAX. SLOPE; MAY BE CURBED	-
CROSS SLOPE AT INTERSECTION CROSSING WITH YIELD OR STOP CONTROL	2% MAX.	10
CROSS SLOPE AT INTERSECTION CROSSING WITHOUT YIELD OR STOP CONTROL (INCLUDING ANY SIGNAL BUT FLASHING RED)	HIGHWAY GRADE IS MAX. (SEE NOTE 3)	10
GRADE (RUNNING SLOPE)	8.33% MAX.	9
GRADE (RUNNING SLOPE), IF SPACE IS LIMITED	10% FOR 6" RISE	9
CLEAR SPACE FOR DIAGONAL RAMPS	48" × 48" MIN.	12, 13
GRATING SPACES (IN WALKING SURFACE)	0.5" MAX.	15
VERTICAL CHANGES	0.5" MAX., WITH 1:2 MAX. BEVEL BETWEEN 0.25" AND 0.5" HIGH	14

NEW YORK STATE OF OPPORTUNITY.	Department of Transportation							
U.S. CUSTOMARY	STANDARD SHEET							
U.S. CUSTOMARY STANDARD SHEET PEDESTRIAN FACILITIES (SHEET 11 OF 12)								
APPROVED AUGUST 18, 2021	ISSUED UNDER EI 21-024							
/S/ RICHARD WILDER, P.E. DEPUTY CHIEF ENGINEER (DESIGN)	608-01							

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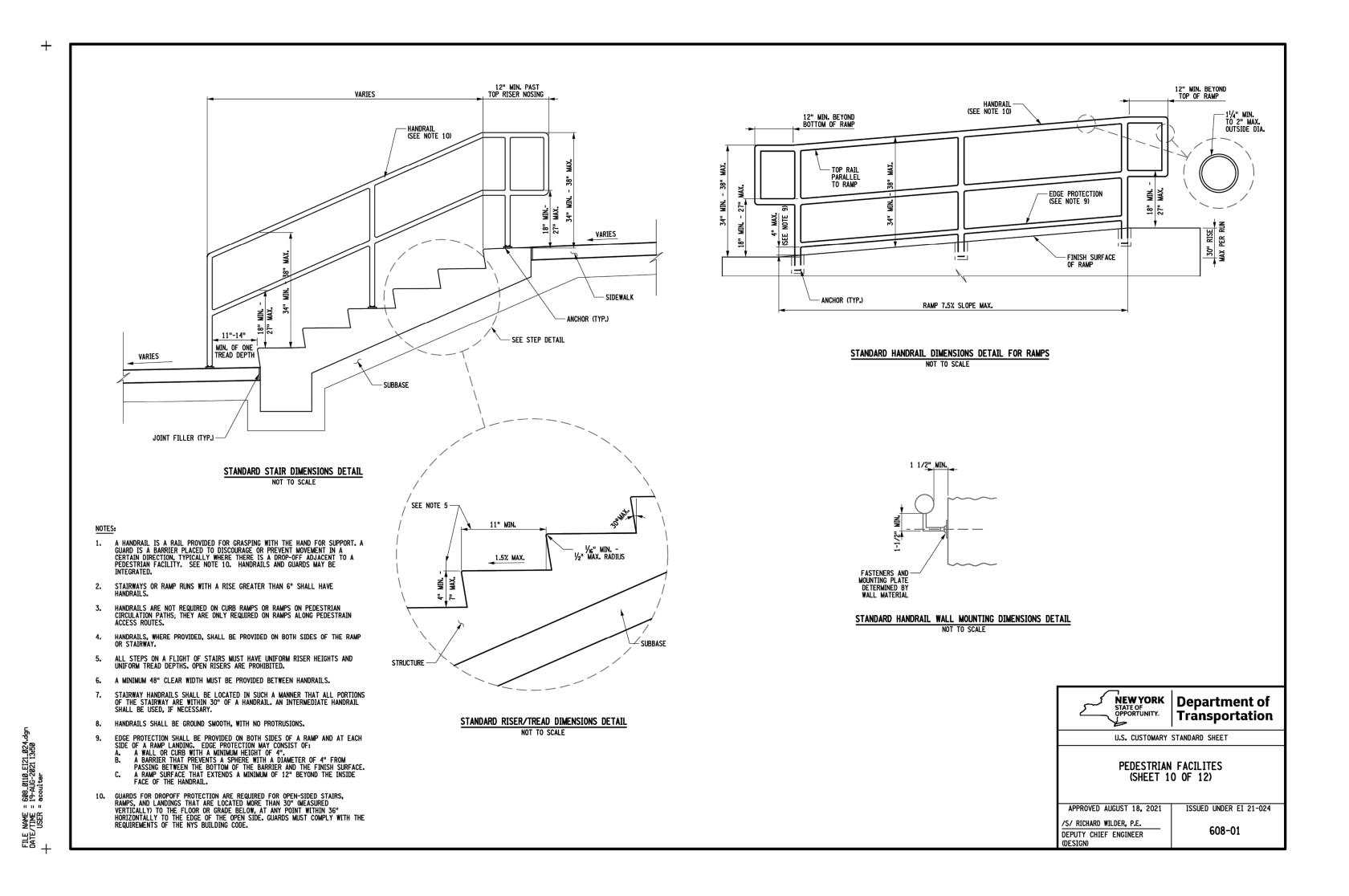
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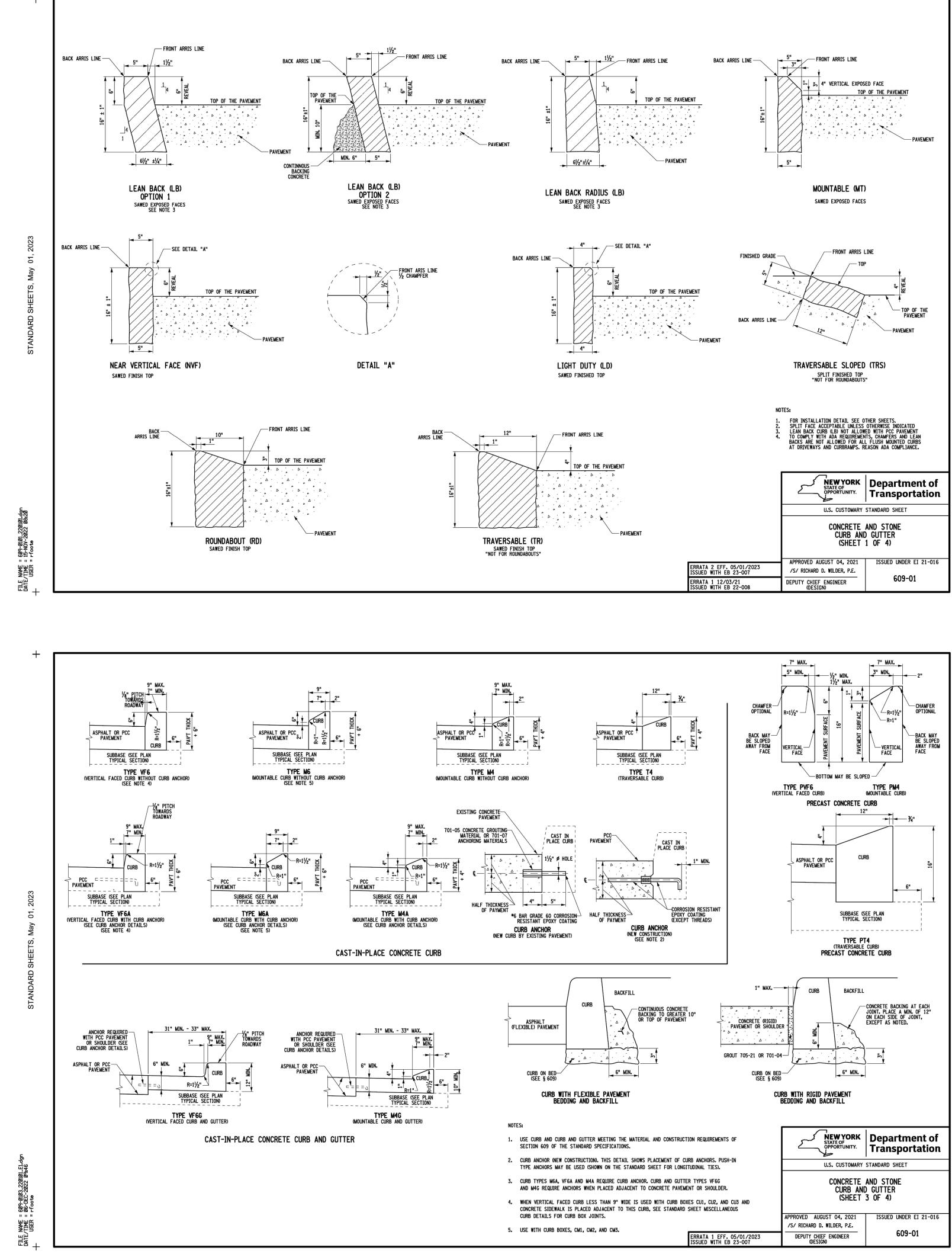
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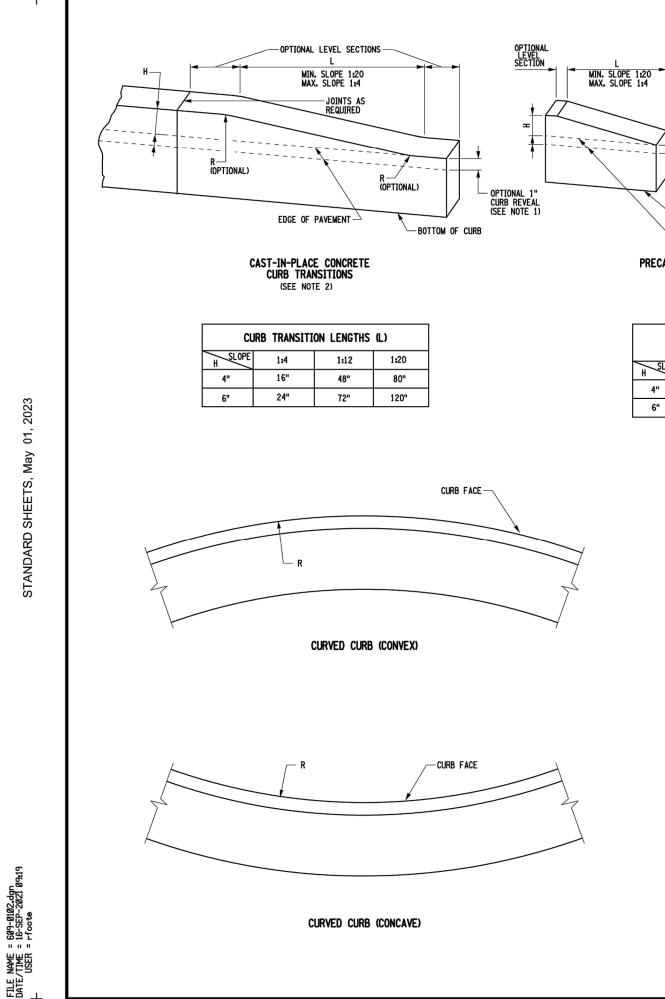
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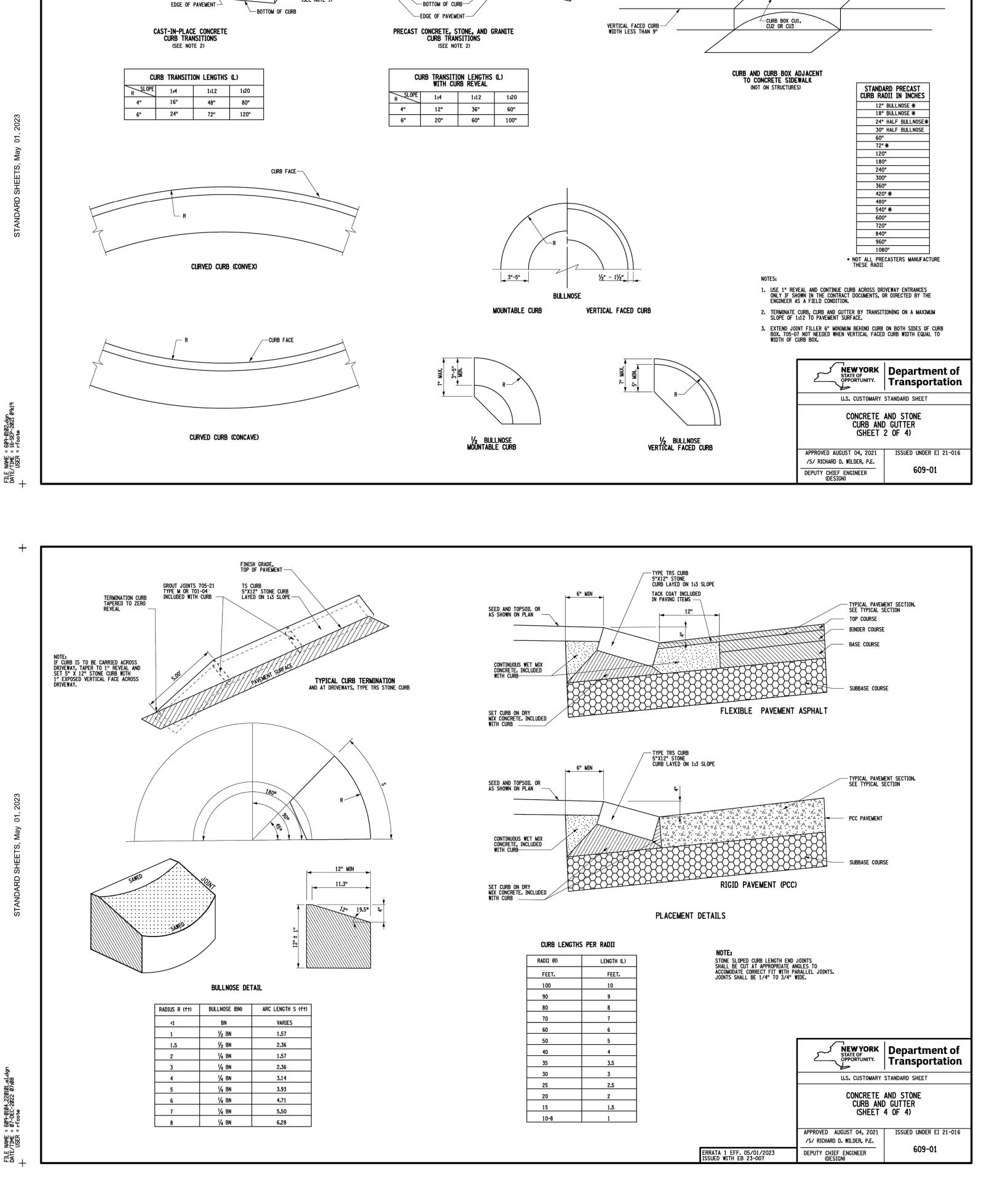


ELEMENT	LIMITS FOR DESIGN AND LAYOUT	LIMITS FOR WORK ACCEPTANCE	INSPECTION METHODS (NOTE NO.)	ELEMENT	LIMITS FOR DESIGN AND LAYOUT	LIMITS FOR WORK ACCEPTANCE	INSPECTION METHODS (NOTE NO.)	ELEMENT	LIMITS FOR DESIGN AND LAYOUT	LIMITS FOR WORK ACCEPTANCE	INSPECTION METHODS (NOTE NO.)
CURB RAMPS/BLENDED TRANSITIONS				CROSSWALKS				STAIRS			
CLEAR WIDTH	48" MIN.	48" MIN.	12	WIDTH	72" MIN.	48" MIN.	12	RISER HEIGHT (MUST BE UNIFORM ON FLIGHT)	4" MIN 7" MAX.	4" MIN 7" MAX.	14
SLOPE OF FLARED SIDES, WITHIN	9.5% MAX.	10% MAX.	10	CROSS SLOPE AT INTERSECTION WITH VIELD OR STOP CONTROL	1.5% MAX.	2% MAX.	10	CLEAR WIDTH OF STAIRS	48" MIN.	44" MIN.	12
PEDESTRIAN CIRCULATION PATH SLOPE OF FLARED SIDES. OUTSIDE	NO MAX. SLOPE. MAY BE	NO MAX. SLOPE, MAY BE						DEPTH OF TREAD (MUST BE UNIFORM ON FLIGHT)	11" MIN.	11" MIN.	13
PEDESTRIAN CIRCULATION PATH	CURBED	CURBED	10	CROSS SLOPE AT PEDESTRIAN CROSSINGS WHERE THERE IS NO YIELD OR STOP SIGN, OR	4.5% MAX.	5% MAX.	10	TREAD SURFACE SLOPE	LEVEL TO 1.5% MAX.	LEVEL TO 2% MAX.	11
GRADE (RUNNING SLOPE) FOR CURB RAMP	7.5% MAX.	8.3% MAX.	9	WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE.		57. max.			0.5" MAX. RADIUS ON LEADING EDGE, 1.25" MAX.	0.5" MAX. RADIUS ON LEADING EDGE, 1.25" MAX.	
GRADE (RUNNING SLOPE) FOR BLENDED	4.5% MAX.	5% MAX.	9	CROSS SLOPE, MIDBLOCK	HIGHWAY GRADE IS MAX.	HIGHWAY GRADE IS MAX.	10	TREAD NOSING	EXTENSION OVER TREAD	EXTENSION OVER TREAD	13
TRANSITION CROSS SLOPE (AT CROSSING WITH YIELD			-	GRADE (RUNNING SLOPE), I.E., HIGHWAY	4.5% MAX.	5% MAX.	9	CLOSED RISER	BELOW YES	BELOW	-
OR STOP CONTROL)	1.5% MAX.	2% MAX.	10	CROSS SLOPE			9	RISE BETWEEN LANDINGS	144" MAX.	144" MAX.	14
CROSS SLOPE (AT CROSSING WHERE THERE IS NO YIELD OR STOP SIGN. OR WHERE THERE IS				MARKINGS	TYPE L, S, LS, DIAGONAL, OR OTHER	TYPE L, S, LS, DIAGONAL, OR OTHER	-		REQ. AT TOP AND BOTTOM	REQ. AT TOP AND BOTTOM	
A TRAFFIC SIGNAL THAT IS DESIGNED FOR	HIGHWAY GRADE IS MAX.	HIGHWAY GRADE IS MAX.	10	CLEAR WIDTH WITHIN MEDIAN OR				LANDING	OF STAIRS	OF STAIRS	-
THE GREEN PHASE). LENGTH OF CURB RAMP. IF MAXIMUM				PEDESTRIAN REFUGE ISLAND	60" MIN.	60" MIN.	12	LANDING WIDTH	MIN. WIDTH EQUAL TO Stairway width	MIN. WIDTH EQUAL TO Stairway width	12
ALLOWABLE SLOPE WILL NOT "CATCH UP"	DOES NOT NEED TO EXCEED 15'-1"	DOES NOT NEED TO EXCEED 15'	13	PEDESTRIAN RAMPS (ALL RAMPS EXCEPT CURB R		201 1/21					
TO GRADE.			40.47	CLEAR WIDTH CROSS SLOPE	36" MIN. 1.5% MAX.	36" MIN. 2% MAX.	12	LANDING LENGTH	MIN. LENGTH EQUAL TO STAIRWAY WIDTH, UP TO 48"	MIN. LENGTH EQUAL TO STAIRWAY WIDTH, UP TO 48"	13
TURNING SPACE, WITH NO CONSTRAINTS TURNING SPACE, WITH CONSTRAINTS AT	48"X48" MIN.	48"X48" MIN.	12, 13	GRADE (RUNNING SLOPE)	7.5% MAX.	8.3% MAX.	9	LANDING SLOPE, IN ANY DIRECTION	1.5% MAX.	2% MAX.	11
BACK OF SIDEWALK	48"X60" MIN.	48"X60" MIN.	12, 13	VERTICAL RISE, BETWEEN LANDINGS	30" MAX.	30" MAX.	14	DRAINAGE			
TURNING SPACE, WITH CONSTRAINTS ON TWO SIDES	48"X60" MIN.	48"X60" MIN.	12, 13	LANDING WIDTH, WITH NO CHANGE OF PED.	WIDTH OF WIDEST RAMP	WIDTH OF WIDEST RAMP	12		NO LOW COOTS THAT WILL	NO LOW CROTC THAT WILL	
SLOPE OF TURNING SPACE AT CONTROLLED	1.5% MAX.	2% MAX.	11	DIRECTION LANDING WIDTH. WITH A CHANGE OF PED.	LEADING TO LANDING	LEADING TO LANDING		ADEQUATE DRAINAGE	NO LOW SPOTS THAT WILL POND WATER WITHIN PED.	NO LOW SPOTS THAT WILL POND WATER WITHIN PED.	-
INTERSECTIONS, IN ANY DIRECTION CROSS SLOPE OF TURNING SPACE AT				DIRECTION	60" MIN.	60" MIN.	12		ACCESS ROUTE	ACCESS ROUTE	
CROSSINGS WHERE THERE IS NO YIELD OR STOP	HIGHWAY GRADE Is Max.	HIGHWAY GRADE IS MAX.	10	LANDING LENGTH LANDING SLOPE, IN ANY DIRECTION	60" MIN. 1.5% MAX.	60" MIN. 2% MAX.	13	PEDESTRIAN SIGNALS			
SIGN, OR WHERE THERE IS A TRAFFIC SIGNAL THAT IS DESIGNED FOR THE GREEN PHASE.	IS MAX.	15 MAX.		RAILINGS AND GUARDS FOR PEDESTRIAN RAMPS.			12	PUSH BUTTON HEIGHT	42" MIN 48" MAX.	42" MIN 48" MAX.	17
CROSS SLOPE OF TURNING SPACE AT MIDBLOCK CROSSINGS	HIGHWAY GRADE IS MAX.	HIGHWAY GRADE IS MAX.	10	RAMP (WITH A RISE OF OVER 6") OR STAIRS	HANDRAIL REQ. ON BOTH SIDES	HANDRAIL REQ. ON BOTH SIDES	-	PUSH BUTTON DISTANCE FROM PEDESTRIAN ACCESS ROUTE DIMENSIONS OF CLEAR SPACE ADJACENT	9" MAX.	10" MAX.	12
COUNTER SLOPE AT BOTTOM OF RAMP	4.5% MAX.	5% MAX.	9	HEIGHT OF HANDRAIL (FROM WALKING				TO PUSH BUTTON	30"X48" MIN.	30"X48" MIN. MATCH GRADE OF	12, 13
CLEAR SPACE (BEYOND BOTTOM GRADE BREAK. OUTSIDE OF PARALLEL VEHICLE	48"X48" MIN.	48"X48" MIN.	12, 13	SURFACE OR STAIR NOSING TO TOP OF GRIPPING SURFACE)	34" MIN 38" MAX.	34" MIN 38" MAX.	14	GRADE (RUNNING SLOPE) OF CLEAR SPACE ADJACENT TO PUSH BUTTON	MATCH GRADE OF ADJACENT PED. ACCESS	ADJACENT PED. ACCESS	9
PATH: CAN INCLUDE DROP CURB)				HEIGHT OF GUARDS, FOR VERTICAL DROPS	42" MIN.	42" MIN.	14	CROSS SLOPE OF CLEAR SPACE	ROUTE	ROUTE	
DETECTABLE WARNINGS Dome Dimensions and spacing	ON DOT APPROVED LIST	ON DOT APPROVED LIST	-	> 30" (SEE NOTE 3, THIS SHEET)	TZ (M1116	72 MIN		ADJACENT TO PUSH BUTTON	1.5% MAX.	2% MAX.	10
CONTRAST OF WARNING DEVICE	LIGHT ON DARK OR DARK ON LIGHT	LIGHT ON DARK OR DARK ON LIGHT	-	OPENING LIMITATIONS FOR GUARDS (SEE NOTE 3. THIS SHEET)	4" MAX. (4¾" ALLOWED FROM A HEIGHT OF 36" TO 42")	4" MAX. (4¾" ALLOWED FROM A HEIGHT OF 36" TO 42")	15	CLEARANCE TIMING	3.5 FT/S MAX. WALKING SPEED	3.5 FT/S MAX. WALKING SPEED	-
ALIGNMENT ON SLOPES > 5%	PERPENDICULAR TO GRADE BREAK BETWEEN RAMP RUN AND STREET	BREAK BETWEEN RAMP RUN AND STREET	-	DISTANCE BETWEEN BOTTOM OF GUARD AND WALKING SURFACE ON RAMP	4" MAX.	4" MAX.	15	1. NOTES REFERENCED ON THIS SHEET ARE I	FOUND ON SHEET 11 OF 12.		
WIDTH	FULL WIDTH OF RAMP OR PAR (2" BORDER ALLOWED)	FULL WIDTH OF RAMP OR PAR (2" BORDER ALLOWED)	12		TRIANGLE FORMED BY	TRIANGLE FORMED BY		2. MORE CRITICAL ELEMENTS FOR PEDESTRIA			
LENGTH (DEPTH)	24" MIN. IN DIRECTION OF PEDESTRIAN TRAVEL ACROSS FULL WIDTH OF RAMP OR P.A.R.	24" MIN. IN DIRECTION OF PEDESTRIAN TRAVEL ACROSS FULL WIDTH OF SIDEWALK RAMP OR P.A.R.	13	DISTANCE BETWEEN BOTTOM OF GUARD AND WALKING SURFACE ON STAIRS	RISER, TREAD, AND BOTTOM RAIL CANNOT ALLOW PASSAGE OF 6" SPHERE	RISER, TREAD, AND BOTTOM RAIL CANNOT ALLOW PASSAGE OF 6" SPHERE	15	LAYOUT AND ACCEPTANCE OF PEDESTRIAN FACILITIES", REFERENCED IN HIGHWAY DESIGN MANUAL CHAPTER 18. 3. DOES NOT APPLY TO HIGHWAY SEGMENTS, HIGHWAY RAMPS, OR HIGHWAY BRIDGES WITH BRIDGE OR HIGHWAY BARRIERS. 4. PEDESTRIAN ACCESS ROUTE (P.A.R.) IS DEFINED ON SHEET 1 OF 12.			
SIDEWALK (SEE NOTE 7)				HANDRAIL CLEARANCE (BETWEEN GRIPPING SURFACE AND WALL)	1.5" MIN.	1.5" MIN.	12				
CLEAR WIDTH OF PED. ACCESS ROUTE (EXCLUDING CURB)	48" MIN.	48" MIN.	12	OUTSIDE DIAMETER OF CIRCULAR	1.25" MIN 2" MAX.	1.25" MIN 2" MAX.	16				
GRADE (RUNNING SLOPE) WHERE HWY. GRADE IS 5% OR LESS	4.5% MAX.	5% MAX.	9	HANDRAIL OUTSIDE PERIMETER OF NON-CIRCULAR HANDRAIL	4" MIN 6.25" MAX.	4" MIN 6.25" MAX.	16				
GRADE (RUNNING SLOPE) WHERE HWY. GRADE IS > 5%	HWY. EDGE OF PVMT. GRADE IS MAX.	HWY. EDGE OF PVMT. GRADE IS MAX.	9	CROSS-SECTION DIMENSION OF NON- CIRCULAR HANDRAIL	2.25" MAX.	2.25" MAX.	16				
CROSS SLOPE	1.5% MAX.	2% MAX.	10	HANDRAIL PLACEMENT ON STAIRS	WITHIN 30" OF ANY POINT	WITHIN 30" OF ANY POINT	12				
PASSING SPACE INTERVAL (IF PED. ACCESS ROUTE IS LESS THAN 60" WIDE)	200' MAX.	200' MAX.	13	HANDRAIL EXTENSION AT TOP OR BOTTOM	ON TREAD 12" MIN. PAST RAMP RUN	ON TREAD	13		NE STAT	WYORK Depart	ment of
PASSING SPACE DIMENSIONS SURFACES	60"X60" MIN.	60"X60" MIN.	12, 13	HANDRAIL EXTENSION AT TOP OF STAIRS	12" MIN. PAST FIRST RISER	12" MIN. PAST FIRST RISER	13				ortation
MATERIAL	FIRM, STABLE, AND SLIP	FIRM, STABLE, AND SLIP	-	HANDRAIL EXTENSION AT TOP OF STAIRS	NOSING MIN. OF ONE TREAD DEPTH	NOSING MIN. OF ONE TREAD DEPTH			U.S.	CUSTOMARY STANDARD SHEE	ET
MATERIAL HORIZONTAL OPENINGS (SUCH AS	RESISTANT	RESISTANT		STAIRS	PAST BOTTOM RISER	PAST BOTTOM RISER	13				
GRATINGS AND JOINTS)	0.5" MAX.	0.5" MAX. 0.5" MAX. WITH 1:2 MAX.	13, 15							DESTRIAN FACILITIES (SHEET 12 OF 12)	5
VERTICAL DISCONTINUITIES	0.25" MAX.	BEVEL BETWEEN 0.25" AND 0.5" High	14						APPROVED AUGUST	18.2021 ISSUED UM	DER EI 21-024
									/S/ RICHARD WILDER.		ULN EI ZI-UZ4









MIN. SLOPE 1:20 MAX. SLOPE 1:4

OPTIONAL 1" CURB REVEAL (SEE NOTE 1)

OPTIONAL 1"
 CURB REVEAL
 (SEE NOTE 1)

── JOINTS %6" WIDE WITH ── 705-07 PREMOULDED RESILIENT JOINT FILLER (SEE NOTE 3)

- THE TYPICAL DETAILS DEPICTED ON THE STANDARD SHEETS AND IN THE MUTCD, REFLECT THE MINIMUM REQUIREMENTS.
- 2. PROPOSED REVISIONS TO THE TRAFFIC CONTROL PLAN SHALL BE PROVIDED, IN WRITING, TO THE DOT ENGINEER FOR REVIEW AND APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE FIVE (5) WORK DAYS PRIOR TO THE PLANNED IMPLEMENTATION OF SUCH PROPOSED REVISIONS, EXCEPT FOR CHANGES THAT ALTER THE SCOPE OF THE TRAFFIC CONTROL PLAN. SUCH CHANGES IN SCOPE MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL BY THE REGIONAL DIRECTOR OR HIS/HER DESIGNEE THIRTY (30) WORK DAYS PRIOR TO IMPLEMENTATION OF SUCH REVISIONS.
- 3. THE NAMES, ADDRESSES, AND TELEPHONE NUMBERS OF STAFF WHO ARE AUTHORIZED TO SECURE LABOR, MATERIALS, AND EQUIPMENT FOR EMERGENCY REPARTS OUTSIDE NORMAL WORKING HOURS SHALL BE PROVIDED, IN WRITING, TO THE NYSDOT ENGINEER. THE ENGINEER WILL PROVIDE THE SUBMITTED INFORMATION TO REGIONAL MANAGEMENT, THE NEW YORK STATE POLICE, THE RESIDENT ENGINEER, AND THE LOCAL POLICE.
- STANDARD SHEET 619-503 MAY BE USED FOR AN OFFSITE DETOUR SETUP FOR BOTH LONG TERM AND SHORT TERM WORK DURATIONS.
- 5. REGIONAL HIGH-VOLUME RESTRICTIONS SHALL BE FOLLOWED. CONSULT WITH DOT ENGINEER IF EXCEPTION NEEDED.
- PLAN AHEAD TO AVOID CONFLICTING WORK ZONES. CHECK FOR CONSTRUCTION PROJECTS, CLOSURES, & RESTRICTIONS AT WWW.511NY.ORG, WWW.DOT.NY.GOV/PROJECTS, AND WITH NYSDOT ENGINEER.
- WORK ZONE INCIDENTS SHALL BE DOCUMENTED AND REPORTED USING EITHER THE DEPARTMENT'S WORK ZONE INCIDENT FORM, OR THE CONSTRUCTION INCIDENT REPORTING SYSTEM, AS APPROPRIATE.
- 8. CONSIDER CLOSURE WIDTH AND THE ABILITY TO ACCOMMODATE WIDE LOAD VEHICLES BEFORE. ESTABLISHING WORK ZONES. 9. IF THE WORK ZONE AFFECTS AN EXISTING ACCESSIBLE AND DETECTABLE PEDESTRIAN FACILITY, ACCESSIBILITY AND DETECTABILITY SHALL BE PROVIDED ALONG THE ALTERNATE ROUTE.
- ACTIVITY AREA A 500' MINIMUM LONGITUDINAL DISTANCE SHALL BE MAINTAINED BETWEEN CONSTRUCTION OPERATIONS ON ALTERNATE SIDES OF THE ROADWAY, UNLESS OTHERWISE APPROVED BY THE ENGINEER.
- 2. WHEN TWO OR MORE AREAS ARE ADJACENT, OVERLAP, OR ARE IN CLOSE PROXIMITY, THE CONTRACTOR SHALL ENSURE THERE ARE NO CONFLICTING SIGNS AND THAT LANE CONTINUITY IS MAINTAINED THROUGHOUT ALL WORK AREAS. SIGNS
- 1. THE LOCATIONS OF THE SIGNS SHOWN ON THE WORK ZONE TRAFFIC CONTROL PLANS AND DETAILS MAY BE ADJUSTED BASED ON SIGHT DISTANCE AND OTHER CONSIDERATIONS. THE FINAL LOCATIONS OF SIGNS ARE SUBJECT TO APPROVAL OF THE ENGINEER.
- 2. FOR LONG TERM WORK DURATIONS, ANY EXISTING SIGNS, INCLUDING OVERHEAD SIGNS, WHICH CONFLICT WITH THE TEMPORARY TRAFFIC CONTROL SIGN LAYOUT SHALL BE COVERED, REMOVED, STORED OR RESET, AS APPROVED BY THE ENGINEER. ALL APPROPRIATE EXISTING SIGNS SHALL BE RESTORED TO THEIR ORIGINAL CONDITION AND/OR LOCATION UNLESS OTHERWISE REPLACED IN THIS CONTRACT.
- SIGNS AT OR NEAR INTERSECTIONS SHALL BE PLACED SO THAT THEY DO NOT OBSTRUCT A MOTORIST'S LINE OF SIGHT. 4. SIGNS MOUNTED ON THE MEDIAN OF DIVIDED HIGHWAYS WHERE MEDIAN BARRIER IS IN PLACE MAY BE MOUNTED ON THE BARRIER WITH A SADDLE TYPE BRACKET OR OMITTED WITH THE APPROVAL OF THE DOT ENGINEER, LAYING THE SIGN DOWN IN A HORIZONTAL POSITION IS NOT PERMITTED.
- 5. THE DIMENSIONS OF WORK ZONE TRAFFIC CONTROL SIGNS ARE DESCRIBED IN THE MUTCD. ANY CHANGES TO THE DIMENSIONS SHALL BE APPROVED BY THE REGIONAL DIRECTOR OR BY HIS/HER DESIGNEE.
- NYR9-12 SHALL BE USED IN PLACE OF NYR9-11 WHEN A REDUCED REGULATORY SPEED LIMIT SIGN IS AUTHORIZED.
- 7. RIGID AND FLEXIBLE "ROLL-UP" SIGNS MAY BE USED FOR MOBILE, SHORT DURATION AND SHORT-TERM STATIONARY WORK. RIGID SIGNS MUST BE MOUNTED AT LEAST 5 FEET ABOVE GRADE (7 FEET WHERE THERE ARE PEDESTRIANS OR PARKED CARS), FLEXIBLE SIGNS SHALL BE MOUNTED AT LEAST ONE FOOT ABOVE GRADE. MESH SIGNS SHALL NOT BE USED. USE RETRO REFLECTORIZED RIGID SIGNS FOR NIGHTIME WORK.

- LANE WIDTHS
- 1. UNLESS AUTHORIZED BY THE ENGINEER, THE MINIMUM LANE WIDTHS FOR WORK ZONE TRAVEL LANES SHALL BE AS FOLLOWS: FREEWAYS AND/OR EXPRESSWAYS IS 11'. THE MINIMUM LANE WIDTH FOR ALL OTHER TYPES OF ROADWAYS IS 10'.
- 2. A WRITTEN NOTE SHALL BE PROVIDED TO THE ENGINEER, A MINIMUM OF 21 CALENDAR DAYS IN ADVANCE OF PERFORMING ANY WORK THAT RESULTS IN THE REDUCED WIDTH OF AN EXISTING ROADWAY, SO THAT THE ENGINEER MAY NOTIFY THE REGIONAL PERMIT ENGINEER IN A TIMELY MANNER.
- 3. IF THE WORK ZONE AFFECTS PEDESTRIANS, A MINIMUM PEDESTRIAN PATHWAY WIDTH OF 5 FEET SHALL BE MAINTAINED UNLESS OTHERWISE AUTHORIZED BY THE ENGINEER. 4. TEMPORARY BICYCLE ACCOMMODATIONS SHALL NOT BE LESS THAN WHAT CURRENTLY EXISTS UNLESS AUTHORIZED BY THE ENGINEER.
- PROTECTIVE VEHICLES
- PROTECTIVE VEHICLES ARE DIVIDED INTO 2 CATEGORIES BASED ON THE GROSS VEHICLE WEIGHT (GVW):

 PROTECTIVE VEHICLE LIGHT (PVL) SHALL HAVE A MINIMUM GVW OF 9,500 LBS. OR GREATER.
 PROTECTIVE VEHICLE HEAVY (PVH) SHALL HAVE A MINIMUM GVW OF 22,000 LBS. OR GREATER.
- 2. IF THE PROTECTIVE VEHICLE ENCROACHES INTO THE TRAVEL LANE, OR IF IT REMAINS. ENTIRELY ON THE SHOULDER OF ANY HIGH SPEED ROAD (≥ 45 MPH), IT SHALL BE EQUIPPED WITH A DEPLOYED TRUCK/TRAILER MOUNTED IMPACT ATTENUATOR (TMIA, SEE TABLE 011-01 ON SHEET 619-011). BALLAST MAY BE USED TO BRING A LIGHTER VEHICLE UP TO THE INDICATED WEIGHT PROVIDED THE BALLAST IS SECURELY CONTAINED WITHIN AN ENCLOSED BODY OR OTHERWISE SECURELY FASTENED TO THE VEHICLE PURSUANT TO FEDERAL MOTOR CARRIER SAFETY ADMINISTRATION (FMCSA) CARGO SECUREMENT RULES, SUCH THAT: 1) THE BALLAST WILL NOT SEPARATE FROM THE VEHICLE UPON IMPACT AND 2) THE BALLAST WILGHT WILL NOT EXCEED THE MANUFACTURER'S GROSS VEHICLE WEIGHT RATING (GVWR), TRUCK/TRAILER MOUNTED IMPACT ATTENUATORS SHALL NOT BE MOUNTED/INSTALLED ON VEHICLES WITH A GROSS WEIGHT (GVW) LESS THAN WHAT IS MINIMALLY REQUIRED BY THE MANUFACTURER OF THE TMIA.
- 3. WHEN A PROTECTIVE VEHICLE(S) IS USED BETWEEN THE WORK VEHICLE (CREW) OR HAZARD AND THE TRAFFIC IN A MOVING OPERATION IT IS REFERRED TO AS A SHADOW VEHICLE(S).
- 4. WHEN A PROTECTIVE VEHICLE(S) IS USED BETWEEN THE WORK VEHICLE (CREW) OR HAZARD AND THE TRAFFIC IN A STATIONARY OPERATION IT IS REFERRED TO AS A BARRIER VEHICLE(S).
- 5. WHEN A PROTECTIVE VEHICLE IS USED IN ADVANCE OF EITHER MOVING OR STATIONARY OPERATIONS TO DISPLAY SIGN MESSAGES IT IS REFERRED TO AS AN ADVANCE WARNING VEHICLE. WHEN SIGNS ARE MOUNTED ON AN ADVANCED WARNING VEHICLE, THEY SHALL NOT OBSTRUCT VISIBILITY OF ANY LIGHTS (TAILLIGHTS OR WARNING LIGHTS), OR SIDE-VIEW MIRRORS ON THE VEHICLE, OR TRUCK MOUNTED ARROW BOARDS.
- 6. IN A MOVING OPERATION OR A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR UP TO 1 HOUR, THE OPERATOR SHALL REMAIN IN THE PROTECTIVE VEHICLE WITH THE SAFETY BELT AND HEADREST PROPERLY ADJUSTED, MAINTAIN VEHICLE SPACING, AND KEEP THE WHEELS ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK, THE PARKING BRAKE SHALL BE SET WHENEVER POSSIBLE. TWO-WAY RADIOS SHOULD BE USED TO COMMUNICATE BETWEEN THE OPERATOR AND THE WORK CREW.
- 7. IN A STATIONARY OPERATION THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR, ONCE THE PROTECTIVE VEHICLE HAS BEEN APPROPRIATELY PLACED, IT SHOULD BE UNOCCUPIED. UNOCCUPIED VEHICLE SHALL BE POSITIONED PARALLEL TO TRAFFIC, PARKING BRAKE SET, PLACED IN 2ND GEAR (MANUAL TRANSMISSIONS ZENGINE OFF) OR PARK / NEUTRAL. (AUTOMATIC TRANSMISSIONS). THE FRONT WHEELS SHALL BE ALIGNED WITH THE LANE STRIPING AND LANE TO MAINTAIN LANE DISCIPLINE AND TO STAY IN LANE IF STRUCK.
- 8. NO WORK ACTIVITY, EQUIPMENT, VEHICLES AND/OR MATERIALS SHALL BE LOCATED BETWEEN THE PROTECTIVE VEHICLE AND THE ACTIVE WORK AREA (ROLL AHEAD DISTANCE).
- 9. DIRECT VERBAL COMMUNICATION BETWEEN THE PROTECTIVE VEHICLES AND THE WORK VEHICLE(S) / EQUIPMENT SHALL BE UTILIZED WHERE AVAILABLE.

CHANNEL IZING DEVICES

- WHERE POSSIBLE ALL CHANNELIZING AND GUIDING DEVICES ARE TO BE PLACED SO AS TO PROVIDE A MINIMUM 2' LATERAL CLEARANCE TO THE TRAVELED WAY. 2. A DROP-OFF OF GREATER THAN 24 INCHES WITHIN 10 FEET FROM THE EDGE OF THE TRAVELED WAY TO REMAIN AT THE END OF THE WORK SHIFT SHALL BE SEPARATED FROM TRAFFIC WITH POSITIVE BARTIER, FOR POSTED SPEED LIMIT OF 45 MPH AND LESS, A DROP-OFF OF GREATER THAN 24 INCHES WITHIN 10 FEET FROM THE EDGE OF THE TRAVELED WAY THAT IS 100 FEET OR LESS IN LENGTH WILL BE ALLOWED WITH CHANNELIZING DEVICES CONSISTING OF DRUMS, EXTRA TALL CONES, OR OVERSIZED VERTICAL PANELS ONLY AT A MAXIMUM SPACING OF 20 FEET FOR SHORT DURATIONS NOR TO EXCEED ONE WORK SHIFT.
- 3. TEMPORARY POSITIVE BARRIER MAY B IN SOME CIRCUMSTANCES, WITH APPRO GUIDANCE FOUND IN THE HIGHWAY DE

PUBLIC ACCESS

1. PROPERTY OWNERS WHOSE DRIVEWAN LEAST 24 HOURS PRIOR TO RESTRI PROPERTIES, AT LEAST ONE DRIVEW RESTORED TO ALL DRIVEWAYS AS S 2. SUITABLE RAMPS SHALL BE INSTAL RESIDENTIAL AND COMMERCIAL DRIV

LANE CLOSURES

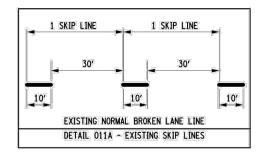
- 1. LANE CLOSURES SHALL BE LOCATED CURVES AND CRESTS, TO THE EXTEN
- 2. THE ENGINEER MAY REQUIRE THAT IS NEEDED FOR EMERGENCY PURPOS OUTSIDE THE CONTRACT LIMITS.
- 3. EACH ARROW PANEL SHALL BE VISIB THE ROADWAY.

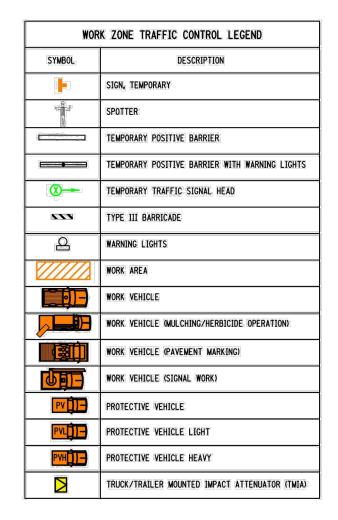
WORK ZONE TRAFFIC CONTROL LEGEND SYMBOL

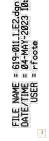
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	ARROW PANEL						
	ARROW PANEL, CAUTION MODE						
	ARROW PANEL TRAILER OR SUPPORT						
Н	CHANGEABLE MESSAGE SIGN (PVMS)						
	CHANNELIZING DEVICE						
4	CONE						
₩	CRASH CUSHION/TEMPORARY IMPACT ATTENUATOR						
	DIRECTION OF TEMPORARY TRAFFIC DETOUR						
¥	DIRECTION OF TRAFFIC						
	AUTOMATED FLAGGER ASSISTANCE DEVICE WITH OPERATOR						
	FLAGGER						
	FLAG TREE						
	LUMINAIRE						
	MOWER						
	PARKWAY GRASS SHOULDER						
11111	PAVEMENT MARKINGS THAT SHALL BE REMOVED FOR A LONG TERM PROJECT						
PVMS	PORTABLE VARIABLE MESSAGE SIGN						
	ADVANCE WARNING SIGN WITH ORANGE FLAGS						
	TRAILER FOR ARROW PANEL OR PORTABLE VARIABLE MESSAGE SIGN (PVMS)						

DESCRIPTION







	APPROVED DECEMBER 21, 2022	ISSUED UNDER EI 22-03
	WORK ZONE TRA General (Sheet	AFFIC CONTROL NOTES 1 OF 2)
	U.S. CUSTOMARY	STANDARD SHEET
	NEW YORK STATE OF OPPORTUNITY.	Department of Transportation
IBLE 1500 FEET IN ADVANCE FROM ANY POINT WITHIN	N	
ALL LANES BE RE-OPENED AT ANY TIME IF THE ROL SES. THIS COULD INCLUDE INCIDENTS AT LOCATIONS	ITE	
D TO PROVIDE OPTIMUM VISIBILITY, I.E. BEFORE INT CONDITIONS PERMIT.		
LED TO MAINTAIN SMOOTH TRANSITIONS FROM VEWAYS TO AND FROM THE WORK AREA.		
YS WILL BE MADE INACCESSIBLE SHALL BE NOTIFIED CTING USE OF THE DRIVEWAY. FOR MULTIPLE ACCESS WAY SHALL BE OPEN AT ALL TIMES. ACCESS SHALL E SOON AS POSSIBLE.	AT	
BE SUBSTITUTED WITH DRUM CHANNELIZING DEVICES, ROVAL OF THE REGIONAL TRAFFIC ENGINEER BASED DESIGN MANUAL AND ENGINEERING JUDGEMENT.	ON	

Robert Limoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM

619-010

ERRATA 2 EFF. 09/01/23 ISSUED WITH EB 23-016

ERRATA 1 EFF. 05/01/23 ISSUED WITH EB 22-033

I	NEW YORK	Department of	
	, Maria de la compañía	Department of Transportation	
	U.S. CUSTOMARY ST WORK ZONE TRAF GENERAL TABLES (SHEET 1	FIC CONTROL	
RRATA 2 EFF. 09/01/23 SUED WITH EB 23-016 RRATA 1 EFF. 05/01/2023 SUED WITH EB 22-033	APPROVED DECEMBER 21, 2022 RobertLinnoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM	ISSUED UNDER EI 22-033 619-011	

									TABLE 011	-01: F	ROTEC	TIVE VEHI	CLE REG	UIREMEN	NTS ^{a,b}												
	DURATIO	N	MOE	BILE OPERA	TION AND STO	P & G0		SHORT DUP	ATION OPERA	TION			SHORT	TERM OPE	RATION												
CLOSURE TYPE	ROAD TYPE &	SPEED	FREEWAY		NON-FREEW		FREEWAY		NON-FREE			FREEWAY	1	0.0000	FREEWAY												
	EXPOSURE CONI			≥ 45 MPH	1 35 - 40 MP	<u>°H ≤ 30 MPH</u>		≥ 45 MPH	35 - 40 N	IPH <u>≤</u>	30 mph		≥ 45 MP	<u>H 35 - </u>	40 MPH												
	WORKERS ON FOU WORK VEHICLE E TO TRAFFIC	OT OR	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL+TMI	A	PVL	PVH+TMIA	PVH+TMI	A PVL	+TMIA												
LANE. CLOSURE OR Encroachment	-NO WORKERS ON -NO WORK VEHIC EXPOSED TO TR/ -OTHER HAZARDS (IE EQUIPMENT,	LE AFFIC 5 EXPOSED	E Contraction of the second seco				PVH+TMIA	PVH+TMIA	PVL		PVL	PVH+TMIA	PVH+TMIA	i PN	VL.												
SHOULDER CLOSURE	WORKERS ON FOU WORK VEHICLE E TO TRAFFIC		PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL		PVL	PVH+TMIA	PVH+TMI	A P	VL												
OR ENCROACHMENT	-NO WORKERS OF -NO WORK VEHIC EXPOSED TO TR/ -OTHER HAZARDS (IE EQUIPMENT, EXCAVATION)	LE AFFIC 5 Exposed	ř				PVH+TMIA	PVH+TMIA	PVL	1	PVL	PVH+TMIA	PVH+TMIA	b PA	VL.												
B. THESE PROTECTIN VEHICLES WITH APPI NECESSARY BY THE	ROPRIATE ROLL A	HEAD DIST	ANCE MAY BE		ADVANCE OF T	HE WORK AREA	IF DEEMED		R LENGTH			OF CONES	CHART	5 . T	Vehicle The Use The Pos												
PRECONSTRUCTION						"H: (FT.)/ * OF	0			DEVICES	P.																
POSTED SPEED LIMIT (MPH)	-4	5	1	6	7	LATERAL SHIFT	OF TRAFF	IC FLOW PA	10 III	1	1	12		4 FT.	5 -												
25	40/1/2	80/2/		/2/3	80/2/3	80/2/3	120/	3/4	120/3/4	120/	30	120/3/4	_	10/1/2	40												
30	80/2/3	80/2/		/2/3	120/3/4	120/3/4	160/		160/4/5	160/	(4/5	200/5/6	_	10/1/2	40												
35	80/2/3 120/3/4	120/3/		0/3/4 0/4/5	160/4/5	160/4/5	200/		200/5/6 280/7/8	240/6/7												240/6/7 320/8/9		240/6/7 320/8/9		10/1/2 10/1/2	40
40	200/5/6	240/6/		0/7/8	320/8/9	360/9/10	400/1	Contraction of the second s	40/11/12	520/13/14		A STREET, STREET, ST				A STREET, STREET, ST		All and a second				560/14/1		0/2/3	80		
50	200/5/6	240/6/	/7 320	0/8/9	360/9/10	400/10/11	440/1	1/12 5	20/13/14	560/14/15		560/14/15		560/14/15		560/14/15		600/15/1	6 8	10/2/3	120						
55	240/6/7	280/7/	/8 320	0/8/9	400/10/11	440/11/12	520/1	3/14 5	60/14/15	600/15/16		680/17/1	8 8	10/2/3	120												
60	240/6/7	320/8/		/9/10	440/11/12	480/12/13	560/1		00/15/16	680/1	040aadd 62002	720/18/1	~ ~ ~	80/2/3	120												
65 • THIS TABLE WAS C •• THE NUMBER OF C DEVICE SPACING, THE	CHANNELIZING DE	VICES SHOW	O MUTCD TAN	LATED BAS			600/1	0/10 0	40/16/17	720/1	8719	800/19/2	<u>n l ö</u>	10/2/3	160												
	011-03					EAD DISTANC	e for pi	ROTECTIVE	VEHICLES	5				TABLE	011-0												
LONGITUDINAL		ЭE		ROLL A	HEAD DISTANC	E (FT.)/# OF SK	IP LINES F	OR VEHICLE	S			0000	ADVANCE	E WARNIN	the second se												
PRECONSTRUCTION POSTED SPEED LIMIT (MPH)	DISTANCE (FT. * OF SKIP LIN	IES	PRECONSTRU POSTED SPEE		9,500 TO 2	EHICLES WEIGHI 1,999 LBS. GVW	22,	000 LBS. 0	HICLES WEIGH R GREATER G	VW		ROAD TYPE A (FT.)															
25	155 / 4		(MPH)		STATIONARY OPERATION	MOVING OPERA (15 MPH MA)		TIONARY RATION	MOVING OPER (15 MPH M)		the second se	AN (≤ 30 MPH AN (35-40 MF		100	100 200												
30	200 / 5	i [≥ 60		200/5	240/6	1	60/4	200/5		URB/	N (≥45 MPH		350	350												
40	305 / 8		45 -	55	160/4	200/5		20/3	160/4		RUR A			500 1000	500 1500												
45	360 / 9	[≤ 40		120/3	120/3		80/2	120/3			ONSTRUCTION															
50	425 / 11					011-05			1		L																
55 65	495 / 13			FLARE	RATES FOR	POSITIVE B			4																		
• THIS TABLE IS TH TABLE 6C-2.			TYF	PE OF POS	ITIVE BARRIER	30	20STED SPE 40 50 MPH MPH	55 65	R																		

 TEMPORARY POSITIVE BARRIER
 8:1
 11:1
 14:1
 16:1
 20:1

 BOX BEAM OR HEAVY POST CORRUGATED BEAM
 7:1
 9:1
 11:1
 12:1
 15:1

1.	THERE ARE MAINLY FIVE WORK DURATIONS:	
	A. LONG-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN 3 CONSECUTIVE DAYS.	
	B. INTERMEDIATE-TERM IS STATIONARY WORK THAT OCCUPIES A LOCATION MORE THAN ONE DAYLIGHT PERIOD UP TO 3 CONSECUTIVE DAYS, OR NIGHTTIME WORK LASTING MORE THAN 1 HOUR.	
	C. SHORT-TERM IS STATIONARY DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.	
	D. SHORT DURATION IS WORK THAT OCCUPIES A LOCATION UP TO 1 HOUR. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES N1 TO N11 NOTES ON NIGHTTIME WORK.	
	E. MOBILE IS WORK THAT MOVES INTERMITTENTLY OR CONTINUOUSLY WHERE THE WORK AT ANY SPECIFIC LOCATION COMPLETES WITHIN 15 MINUTES. IT IS USED FOR VEHICLE BASED OPERATIONS AND DOES NOT INVOLVE WORKERS ON FOOT. IT CAN BE PERFORMED DURING THE DAYTIME OR AT NIGHT IN ACCORDANCE WITH NOTES NI TO NIO NOTES ON NIGHTTIME WORK.	
2.	SPECIAL OPERATIONS ARE WORK OPERATIONS THAT DO NOT FIT INTO ONE OF THE ABOVE FIVE CATEGORIES. SPECIAL OPERATIONS INCLUDE:	
	A. STOP AND GO OPERATIONS - WORK THAT COMPLETES WITHIN 5 MINUTES AND ALLOWS WORKERS ON FOOT.	
	B. OTHER OPERATIONS INCLUDING MOWING, MULCHING/HERBICIDE OPERATIONS, TEMPORARY ROAD/INTERSECTION CLOSURES, ETC.	
ROA	DWAY TYPE DEFINITIONS	
1.	FREEWAY:	
	A. INTERSTATE: INTERRECIONAL HIGH-SPEED, HIGH-VOLUME, DIVIDED FACILITIES WITH COMPLETE CONTROL OF ACCESS.	
	B. PARKWAY: DIVIDED HIGHWAYS FOR NON-COMMERCIAL TRAFFIC WITH FULL CONTROL OF ACCESS, GRADE PARKWAY SEPARATIONS, INTERCHANGES, AND OCCASIONAL AT- GRADE INTERSECTIONS. PARKWAYS ARE DESIGNATED BY LAW.	
2.	EXPRESSWAY: DIVIDED HIGHWAYS FOR THROUGH TRAFFIC WITH FULL OR PARTIAL CONTROL OF ACCESS AND GENERALLY WITH GRADE SEPARATIONS AT MAJOR CROSSROADS. ALL FREEWAY STANDARD SHEETS ARE APPLICABLE TO EXPRESSWAY.	
3.	NON-FREEWAY:	
	A. MULTILANE DIVIDED HIGHWAY	
	B. MULTILANE UNDIVIDED HIGHWAY	
	C. TWO-LANE TWO-WAY ROADWAY	
	ALL NON-FREEWAYS CAN BE EITHER URBAN OR RURAL	
	URBAN: IMEETS MORE THAN 1 OF THE FOLLOWING CRITERIA) *HIGH DENSITY DEVELOPMENT *ON-STREET PARKING *VARIED BUILDING SETBACKS *MULTI-STORY AND LOW-TO MEDIUM-RISE STRUCTURES FOR RESIDENTIAL *COMMERCIAL, AND EDUCATIONAL USES, STRUCTURES THAT ACCOMMODATE MIXED USES: COMMERCIAL, RESIDENTIAL, AND PARKING *LIGHT INDUSTRIAL, AND ISOMETIMES HEAVY INDUSTRIAL, LAND USE *PROMINENT DESTINATIONS WITH SPECIALIZED STRUCTURES, E.G., LARGE THEATERS, SPORTS FACILITIES OR CONFERENCE CENTERS *HIGH LEVELS OF PEDESTRIAN AND BICYCLIST ACTIVITY, WITH NEARLY CONTINUOUS SIDE WALKS AND MARKED CROSSWALKS *HIGHER DENSITIES GREATER THAN 25 DRIVEWAYS/MILE ON EACH SIDE OF THE ROAD *MINOR COMMERCIAL DRIVEWAY DENSITIES OF 10 DRIVEWAYS/MILE OR GREATER *MAJOR COMMERCIAL DRIVEWAYS	
	•HIGH DENSITY OF CROSS STREETS RURAL: DOES NOT MEET MORE THAN ONE OF THE ABOVE CRITERIA.	

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NEW YORK STATE OF OPPORTUNITY.	Department of Transportation							
U.S. CUSTOMARY ST	ANDARD SHEET							
WORK ZONE TRAFFIC CONTROL GENERAL NOTES (SHEET 2 OF 2)								
APPROVED DECEMBER 21, 2022	ISSUED UNDER EI 22-033							
Robert Limoges ROBERT LIMOGES, P.E. DIRECTOR, OTSM	619-010							

E١	AY	FREEWAY		NON-FREEWAY		FREEWAY	-	NON-FREEWAY		FREEWAY		NON-FREEWAY		
M	H <u>≤</u> 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		≥ 45 MPH	35 - 40 MPH	≤ 30 MPH		2 45 MPH	35 - 40 MPH	≤ 30 MPH	
AIN	PVL	PVH+TMIA	PVH+TMIA	PVL+TMIA	PVL	PVH+TMIA	PVH+TMIA	PVL+TMIA	SEE Note 2	PVH+TMIA	PVH+TMIA	PVL+TMIA	SEE NOTE 2	
	PVL	PVH+TMIA	PVH+TMIA	PVL	SEE Note 2	PVH+TMIA	PVH+TMIA	SEE Note 2	SEE Note 2	PVH+TMIA	PVH+TMIA	SEE Note 2	SEE Note 2	
	PVL	PVH+TMIA	PVH+TMIA	PVL	PVL	PVH+TMIA	PVH+TMIA	SEE Note 2	SEE Note 2	PVH+TMIA	PVH+TMIA	SEE Note 2	SEE NOTE 2	
	PVL	PVH+TMIA	PVH+TMIA	PVL	SEE Note 2	PVH+TMIA	SEE NOTE 3	SEE NOTE 2	SEE Note 2	PVH+TMIA	SEE NOTE 3	SEE Note 2	SEE NOTE 2	
				 2. EITHER A 3. EITHER A 4. TRUCK/TR 	PROTECTIVE PROTECTIVE AILER MOUN	LIGHT (PVL Vehicle Hi Fed impact) OR THE S EAVY (PVH) (ATTENUATOR	NO POSITIVE PF FANDARD BUFFER DR THE STANDAR S (TMIA) SHALL INIMALLY REQUIF	SPACE (SE D BUFFER S NOT BE MO	E TABLE 01 PACE (SEE UNTED/INSTA	TABLE 011-C	3) SHALL BE PR HICLES WITH A		
				5. THE USE	OF A PROTE	CTIVE VEHIC	LE LIGHT (P	VL) AS A SHADO SS otherwise	W VEHICLE	IS LIMITED	TO NON-FREE	10 1	WHERE	
HS	& NUMBER	OF CONES	CHART											
GI	EVICES					(FT.)/ Cł	IANNELIZING	LINES/ * OF DEVICES						
Î	n	12	٤ 4	FT. 5-	7 FT.	8 FT.	OR SHOULDE		1	1 हा.	12 FT.	_		
╈	120/3/4	120/3/4	00	8 KD - E	1/2	40/1/2	40/1/	2 U 60 H C	20 C	0/1/2	40/1/2	-		
$^{+}$	160/4/5	200/5/6	_		1/2	40/1/2	40/1/2			0/2/3	80/2/3	-		
t	240/6/7	240/6/7			/1/2	80/2/3	80/2/			0/2/3	80/2/3			
Ĵ	320/8/9	320/8/9	40/	1/2 80/	/2/3	80/2/3	80/2/	3 120/3/	4 12	20/3/4	120/3/4			
	520/13/14	560/14/1	5 80/	2/3 80/	/2/3	120/3/4	120/3/	4 120/3/	/4 1/	20/3/4	160/4/5			
	560/14/15	600/15/1	6 80/	2/3 120	/3/4	160/4/5	160/4/	5 160/4/	/5 10	50/4/5	160/4/5			
	600/15/16	680/17/1	B 80/	2/3 120	/3/4	160/4/5	160/4/	5 160/4/	/5 20	00/5/6	200/5/6			
ļ	680/17/18	720/18/1		60 No. 100-224	/3/4	160/4/5	200/5/	0 10-10-10-10-10-10-10-10-10-10-10-10-10-1	\	10/6/7	240/6/7			
	720/18/19	800/19/2	0 80/	2/3 160	/4/5	200/5/6	240/6/	7 240/6/	7 28	30/7/8	280/7/8	_		
													P	
ES		DOT N	ADVANCE	ABLE 011-06 WARNING SIGN	SPACING			TA	PER LENG	BLE 011-0 Th For T Control	[EMPORAR)			
GH G	NG	ROAD TYPE		ISTANCE BETWEE	1	SIGN LEGE		1000134-10)F TAPER	Î	TAPER L	Ength (L)		
R/		N (≤ 30 MPH		100 100				ERGING TAPER			L L/	2	2	
MA		N (35-40 M	PH+)	200 200	200	AHEAD AI	IEAD S	HOULDER TAPER		THORS	L/	3		
/5 /4	URBA	<u>n (≥45 mph</u> L	- W	350 350 500 500				DNE-LANE, TWO-W DOWNSTREAM TAP				-100 FT. MAX -100 FT. MAX	3	
/4	FREE PRECC ** THIS	ONSTRUCTION	POSTED SP	1000 1500 EED LIMIT. S MUTCD TABLE	<u>.</u>	1 MILE /2	MILE	THIS TABLE IS	THE SAME	AS MUTCD T	ABLE 6C-3.		9	NEW YORK STATE OF OPPORTUNITY. Department of Transportation
	La destan and	Lange of the light	ne constant På	and a share and and	and a second second different of									U.S. CUSTOMARY STANDARD SHEET
														WORK ZONE TRAFFIC CONTROL GENERAL TABLES AND LEGEND (SHEET 2 OF 2)
											E	RATA 2 EFF. SUED WITH EB RRATA 1 EFF. SUED WITH EB	05/01/2023	APPROVED DECEMBER 21, 2022 ISSUED UNDER EI 22-033 RobertLinoges 619-011 ROBERT LIMOGES, P.E. DIRECTOR, OTSM

LONG TERM OPERATION

. PRIOR TO THE START OF NIGHTTIME OPERATIONS, A WRITTEN NIGHTTIME OPERATIONS AND LIGHTING PLAN IS REQUIRED FOR APPROVAL FROM THE DOT ENGINEER. NIO. SEE STANDARD SPECIFICATIONS §619 FOR ADDITIONAL REQUIREMENTS AND CONSIDERATIONS. REFER TO SECTION 619-3.19B FOR BALLOON LIGHTING REQUIREMENTS. FLAGGERS SHALL USE A FLASHLIGHT WITH RED GLOW CONE/RED LED BATON FOR FLAGGING IN NON-ILLUMINATED FLAGGER STATIONS DURING NIGHTTIME OPERATIONS.

INTERMEDIATE TERM OPERATION

ALL LIGHTING SHALL BE DESIGNED, INSTALLED, AND OPERATED TO AVOID GLARE THAT AFFECTS TRAFFIC ON THE ROADWAY OR THAT CAUSES ANNOYANCE OR DISCOMFORT FOR RESIDENCES ADJOINING THE ROADWAY.

LEVEL III ILLUMINATION SHALL BE PROVIDED FOR PAVEMENT OR STRUCTURAL CRACK FILLING, JOINT REPAIR, PAVEMENT PATCHING AND REPAIRS, INSTALLATION OF SIGNAL EQUIPMENT OR OTHER ELECTRICAL/MECHANICAL EQUIPMENT, AND OTHER TASKS INVOLVING FINE DETAILS OR INTRICATE PARTS AND EQUIPMENT.

. LEVEL II ILLUMINATION SHALL BE PROVIDED FOR FLAGGING STATIONS, ASPHALT PAVING, MILLING, AND CONCRETE PLACEMENT AND/OR REMOVAL OPERATIONS, INCLUDING BRIDGE DECKS, 50 FEET AHEAD OF AND 100 FEET BEHIND A PAVING OR MILLING MACHINE.

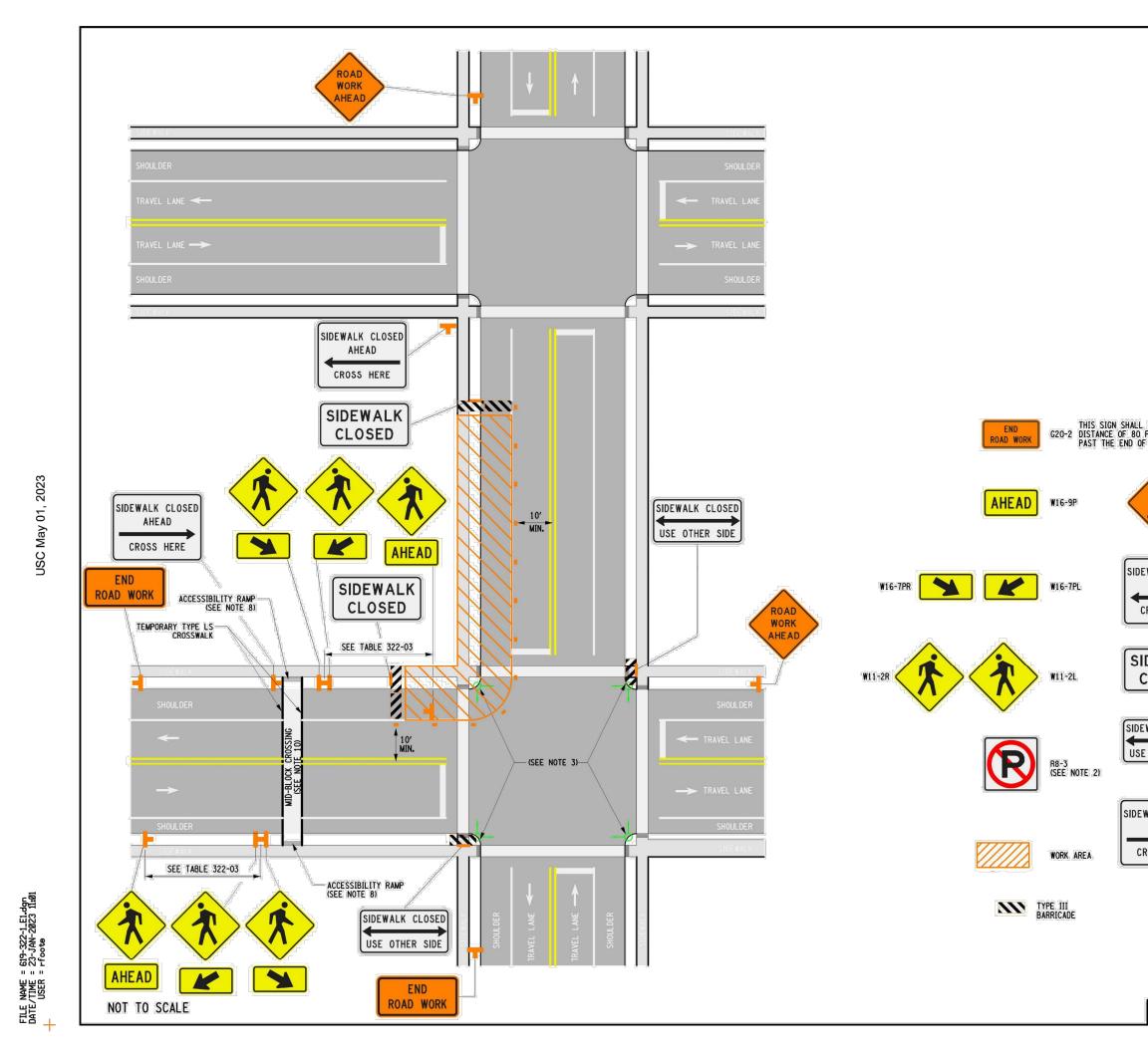
5. LEVEL I ILLUMINATION SHALL BE PROVIDED NEAR THE BEGINNING OF LANE CLOSURE TAPERS AND AT ROAD CLOSURES, INCLUDING THE SETUP AND REMOVAL OF THE CLOSURE TAPERS.

. VEHICLES OPERATING ON THE PAVEMENT OF A CLOSED ROADWAY OR TRAVEL LANE SHALL DISPLAY ROTATING AMBER BEACONS OR FLASHING LED BEACONS AT ALL TIMES.

ALL WORKERS INVOLVED SHALL WEAR PROTECTIVE HELMETS AND NIGHTTIME APPAREL IN ACCORDANCE WITH §107-05A. HIGH VISIBILITY APPAREL AT ALL TIMES.

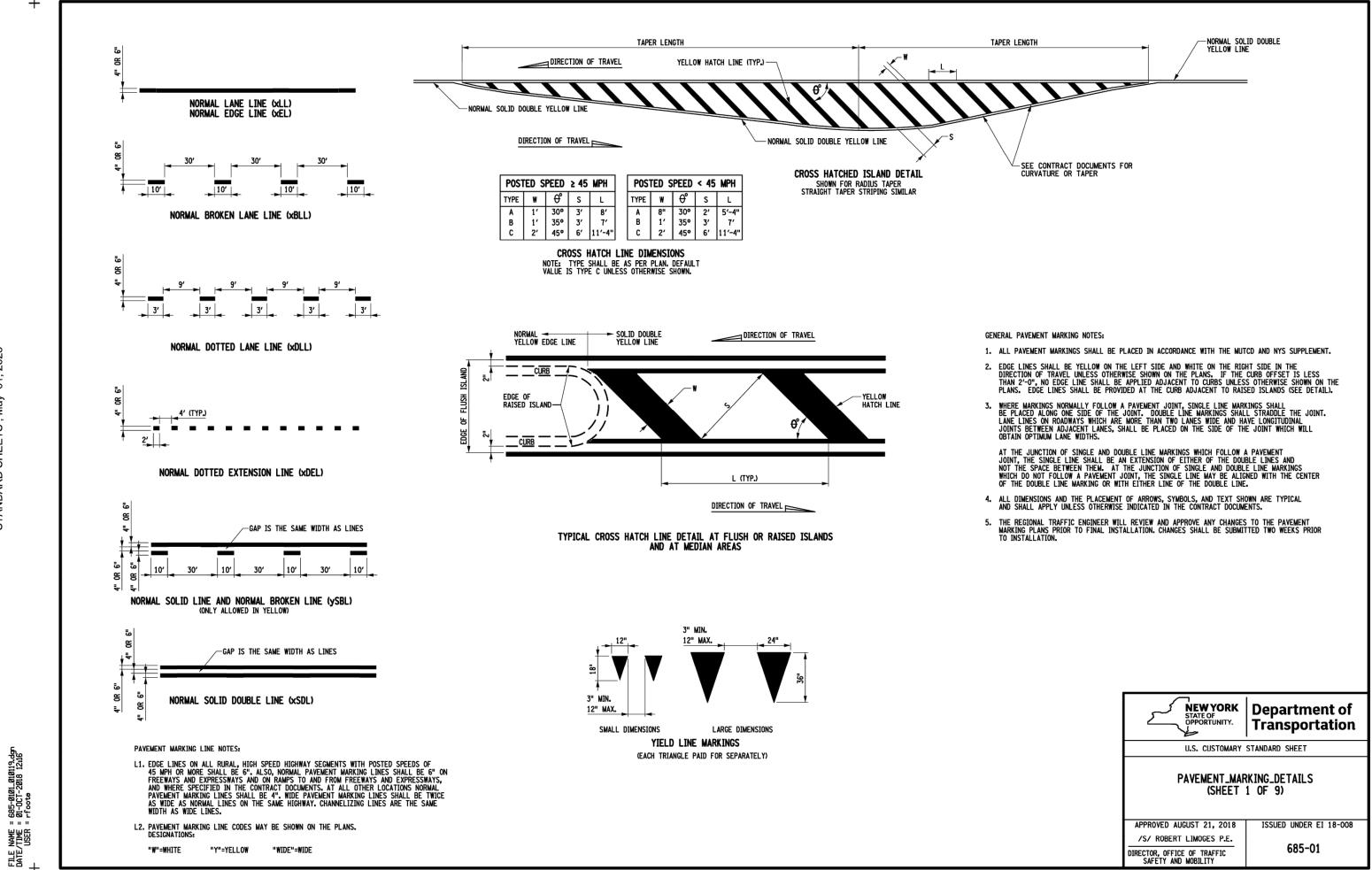
ALL SIGNS, STOP/SLOW PADDLES AND RED FLAGS USED TO WARN/ALERT/CONTROL TRAFFIC SHALL BE RETROREFLECTIVE.

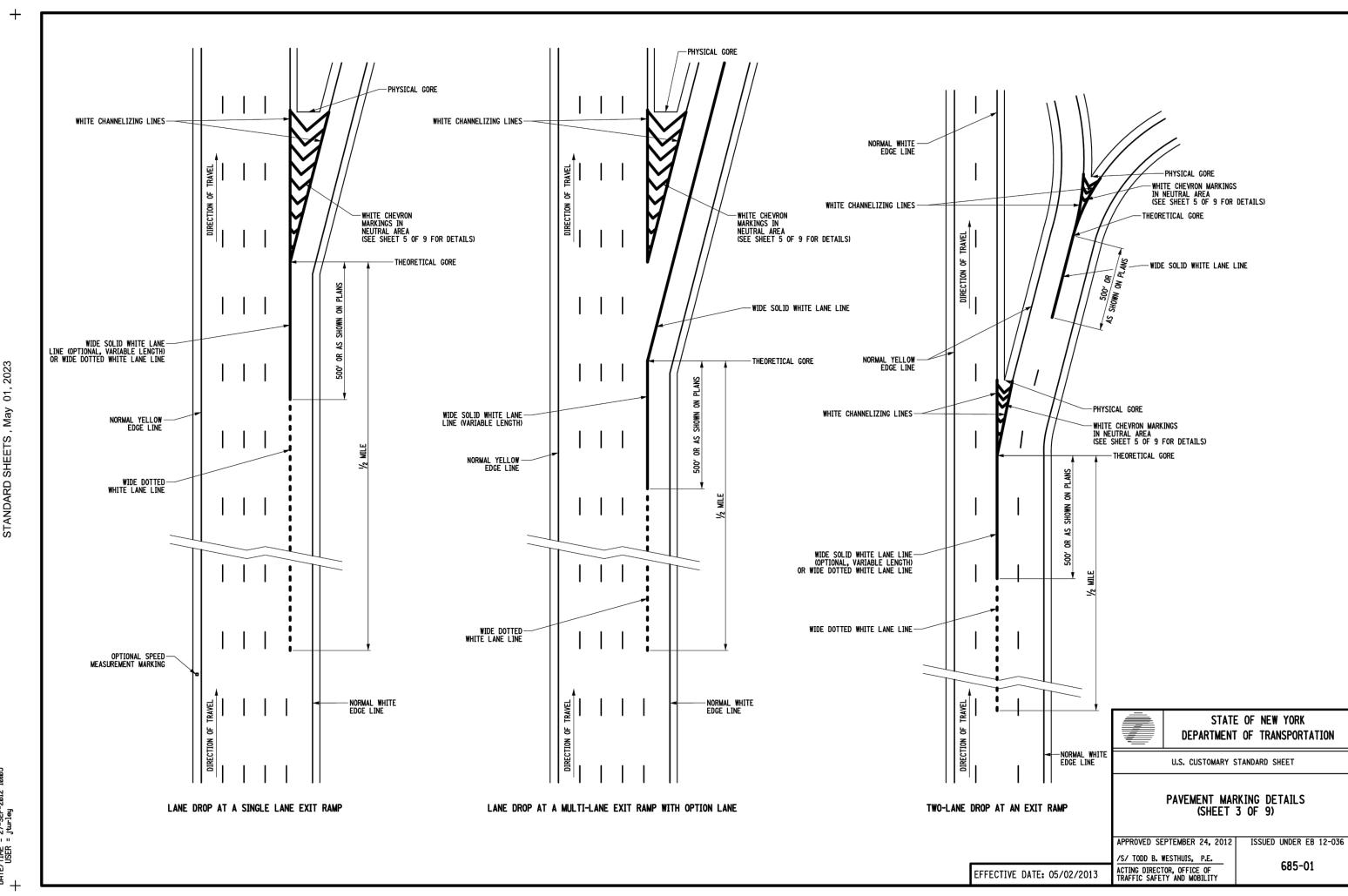
OTES FOR NIGHTTIME OPERATIONS: WORK OCCURRING AFTER SUNSET AND BEFORE SUNRISE WILL BE CONSIDERED NIGHTTIME OPERATIONS.



NOTES:													
1. SHORT-TERM STATIONARY IS DAYTIME WORK THAT OCCUPIES A LOCATION FOR MORE THAN 1 HOUR WITHIN A SINGLE DAYLIGHT PERIOD.		TABLE 32	22-01: REQUIR	ED SIG	SN SIZ	ÆS•							
2. PARKING ALONG THE CURB SHALL BE PROHIBITED FOR AT LEAST 50' IN ADVANCE OF THE MID-BLOCK CROSSWALK. IF THE SHOULDER IS GREATER THAN OR EQUAL TO 8 FOOT, AN		SIGN	NON-FREEWA	Ŷ	5	FREEWAY							
R8-3 SIGN IS RECOMMENDED TO PREVENT PARKING ON SHOULDER. 3. PEDESTRIAN TRAFFIC SIGNAL DISPLAYS CONTROLLING CLOSED CROSSWALKS SHOULD		G20-2	36x18 24x24			48x24 48x48	;						
BE COVERED OR DEACTIVATED.		R8-3 R9-9**	24x24 24x12			24x12							
4. ONLY THE WORK ZONE TRAFFIC CONTROL DEVICES RELATED TO PEDESTRIANS ARE SHOWN. OTHER DEVICES, SUCH AS LANE/SHOULDER CLOSURE SIGNING, ROAD NARROWS SIGNS NEED NO NORTHING STUDIES TO CONTROL NEED TO CONTROL NOT TAKEN AND TAKE		R9-10••	24×12			24x12							
(W5-4), OR NO PARKING SIGNS MAY BE USED TO CONTROL VEHICULAR TRAFFIC. 5. ANY FEATURES CONSTRUCTED AS PART OF A PEDESTRIAN DETOUR MUST BE ADA COMPLIANT		IL/R9-11R**	24×18			24x18							
AND FOLLOW THE 2011 PUBLIC RIGHTS-OF-WAY ACCESSIBILITY GUIDELINES (PROWAG). TEMPORARY RAMPS AND OTHER TEMPORARY PEDESTRIAN FACILITIES SHALL BE PAID FOR		R11-2	48x30			48x30	_						
UNDER THE APPROPRIATE ITEM FOR ASPHALT SIDEWALKS. 6. PROWAG SECTION R205 AND MUTCD PART 6 SHALL APPLY TO ALL TEMPORARY ALTERNATIVE		16-7PR**	24x12										
PEDESTRIAN PATHWAYS.	W11-	-2R/W11-2L	36x36			Ξ							
 TYPE III BARRICADES SHALL BE THE FULL WIDTH OF THE PATH BEING CLOSED. THE GRADE (RUNNING SLOPE), AND CROSS SLOPE OF A TEMPORARY CURB RAMP FOR DESIGN 		W20-1	36x36			48x48							
AND LAYOUT SHALL BE IN ACCORDANCE WITH STANDARD SHEET 608-01.		CONSTRAINTS DO	5 MAY BE USED O D NOT EXIST. R FREEWAY USE.	IN NON-F	REEWA	(, IF SPACE							
9. ACCESS TO BUSINESS ENTRANCES AND TRANSIT STOPS WILL NEED TO BE MAINTAINED. IF THE PEDESTRIAN FACILITY CURRENTLY HAS A TRANSIT STOP THAT WILL BE AFFECTED BY THE WORK ZONE, PRACTITIONERS SHOULD CONSULT THE TRANSITS AUTHORITIES THAT SERVICE THE AFFECTED STOP TO DETERMINE HOW TO ACCOMMODATE THE STOP.			NE BREEMAT USE.										
10. ALL MID BLOCK CROSSWALKS SHALL BE APPROVED BY THE DOT REGIONAL DIRECTOR OR HIS/HER DESIGNEE.		TABLE 3	322-02: CHAN	NELIZIN	IG DE	VICE APP	LICATI	on fof	SHORT	-TERM	STATI	DNARY N	VORK ZON
HIS/HER DESIGNEL.	WOF	rk zone provis	IONS 2		T			MUTCD	COMPLIAN	IT CHANN	IELIZING	DEVICE	1
SHALL BE LOCATED A MINIMUM F 80 FT, AND MAXIMUM OF 400 FT. END OF THE DOWNSTREAM TAPER	STATJ INVOL OCCU FOR	T-TERM IONARY WORK ZC VE WORK THAT PIES A LOCATIO MORE THAN 1 H	N UNIXEN		DRUMS	STANDARD CONES	TALL CONES	EXTRA TALL CONES	TEMPORARY		INTERIM Tubular Markers	VERTICAL PANELS	OVERSIZED VERTICAL PANELS
ROAD WORK W20-1	PERIC	IN A SINGLE DA DD.	20 F	6	x	X	X	x	j e			x	x
SIDEWALK CLOSED AHEAD R9-11L		TABL	.E 322-03: GL	JIDEL IN		OR ADVAN	13540 11 1245			ARNING	i SIGNS		
CROSS HERE	POST	ED CONDIT	COND	ITION B.	0.00 4.0.0	ERATION TO				PEED (M	PH) FOR	THE CON	
CR033 HERE	OR 8 PERCEN	5"- SPE	ED			EED REDUCT					m ron		
SIDEWALK R9-9	SPEC (MP)	AND I				15 20	25	30		0 45	50	55	
CLOSED	20	410) 115	110 1	105	90 75		æ	-	a k	=	a	
	25	1	1			135 120	Des Cul	-	-			-	
SIDEWALK CLOSED	30	020	u	Luis-323 1		185 165 235 215		J (10 (222)	130				
USE OTHER SIDE	40	- Limie	0 01044	Carlor I I	131860	295 280	100 0 100	1.000	ESSEN 64	50 -		æ	
	45	930	D 360	380	370	360 340	315	285	255 2	10 16	5 -	-	
	50	· Erana		cas a		435 415	Cris cii	1. 37,00		85 24	10 000	-	
SIDEWALK CLOSED REFER TO SHEET 2 OF 2 FOR ALL TABLES	55		5 495	530	520	505 490	460	435	400 3	55 31	ວັ 255	205	Î
AHEAD R9-11R NEW YORK Department of	NOTES 1		HAVE NOT BEEN	MODIEIE	DTO	CCOUNT FO	R SIGN	I FGIBII I	TY				
CROSS HERE NEW YORK STATE OF OPPORTUNITY. Department of Transportation U.S. CUSTOMARY STANDARD SHEET	– 2. CHANC RIGHT	TYPICAL CONDIT	TIONS ARE LOCAT EAVY TRAFFIC BE HE DISTANCES AF	IONS WHE	ERE TH F A CO	E ROAD USI	ER MUST /ING SIT	USE E)	TRA TIM TYPICAL	SIGNS A	RE MERG	E AND	
CHANNELIZING DEVICE WORK ZONE TRAFFIC CONTROL		TYPICAL COND	ITION IS THE WAI AHEAD, AND IN	RNING OF		TENTIAL ST	OP SITU	IATION. T	YPICAL S	IGNS AR	e stop	AHEAD,	
(SEE TABLE 322-02) TWO-LANE TWO-WAY ROADWAY CROSSWALK CLOSURE AND PEDESTRIAN DETOUR SHORT TERM OPERATIONS (SHEET 1 OF 2)	1-2-2- 2-2- 2-2- 2-2- 	TYPICAL COND UGH THE WARNEI INCES ARE DETE	ITIONS ARE LOCA D CONDITION. TYP RMINED BY PROV	TIONS WH PICAL SIG IDING A	HERE T GNS AF 2.5 SE	HE ROAD U E TURN, CU COND PRT	SER MUS IRVE, RE AND A V	verse t /ehicle	URN, OR DECELERA	REVERSE	CURVE. TE OF 1	0 FT./SE	COND ²
APPROVED APRIL 8, 2022 ISSUED UNDER EI 22-008 RobertLimoges 619-322		OUT AN ADVISOR			- 90000						- CE (210)		
ERRATA 1 EFF. 05/01/2023 ROBERT LIMOGES, P.E. ISSUED WITH EB 23-007 DIRECTOR, 015M													;
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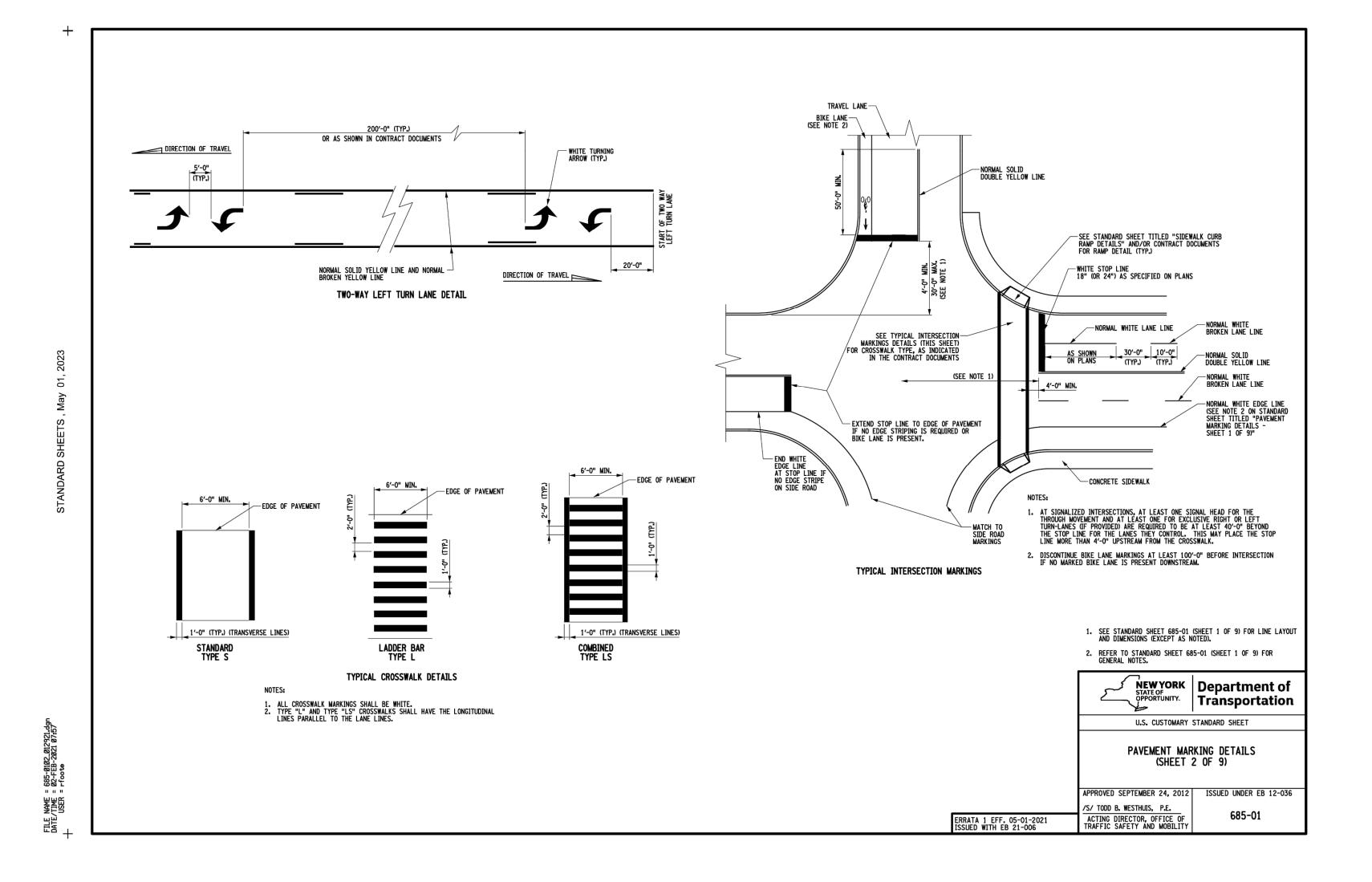
ZONES	3		
VERTICAL PANELS	TYPE III BARRICADES		
x	<u> </u>		
		U.S. CUSTOMARY STANDARD SHE WORK ZONE TRAFFIC CONT TWO-LANE TWO-WAY ROAD CROSSWALK CLOSURE AND PEDEST SHORT TERM OPERATION (SHEET 2 OF 2) APPROVED APRIL 8, 2022 ISSUED L	Ement of Fortation TROL WAY RIAN DETOUR VS UNDER EI 22-008 9-322

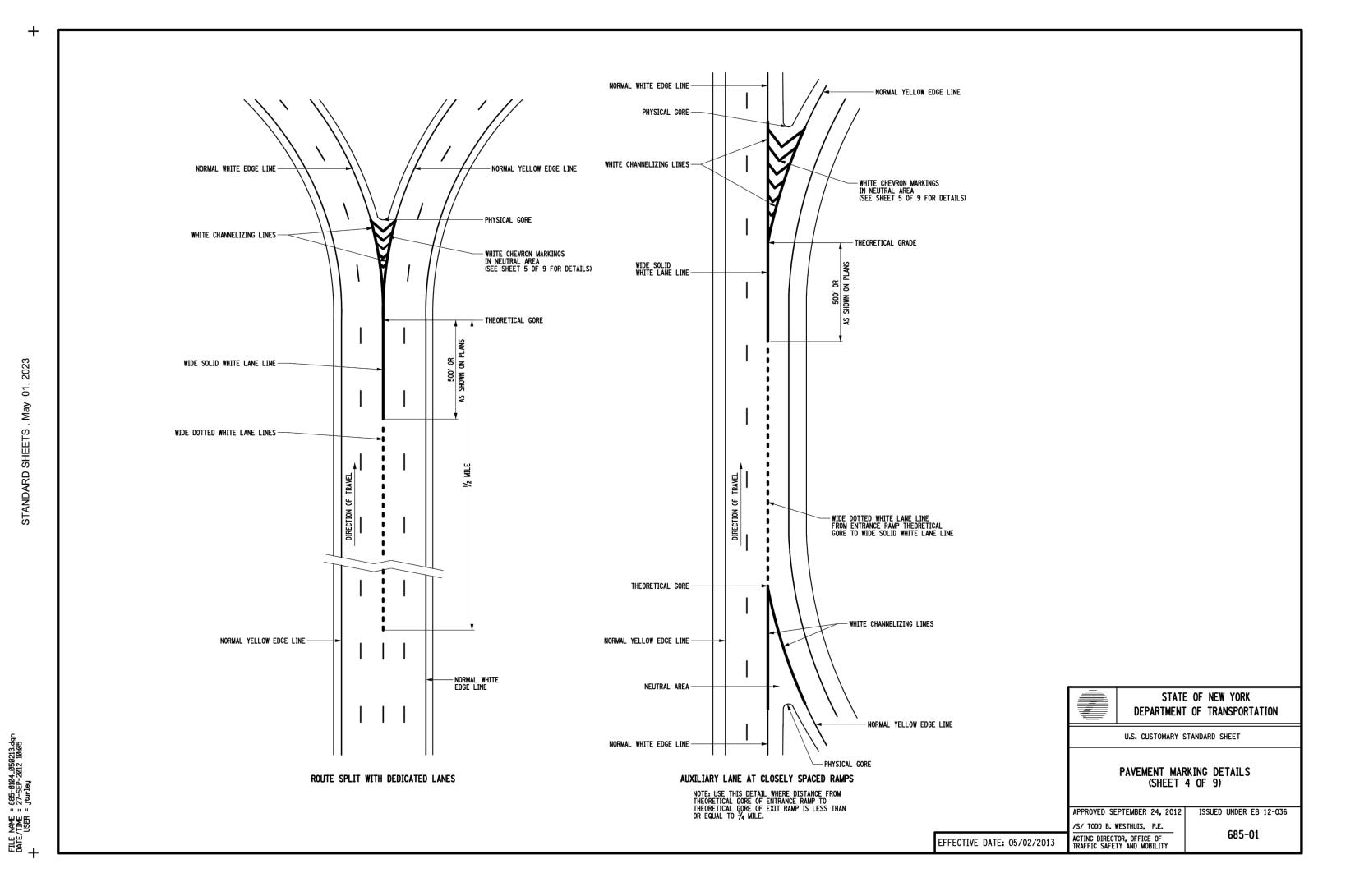


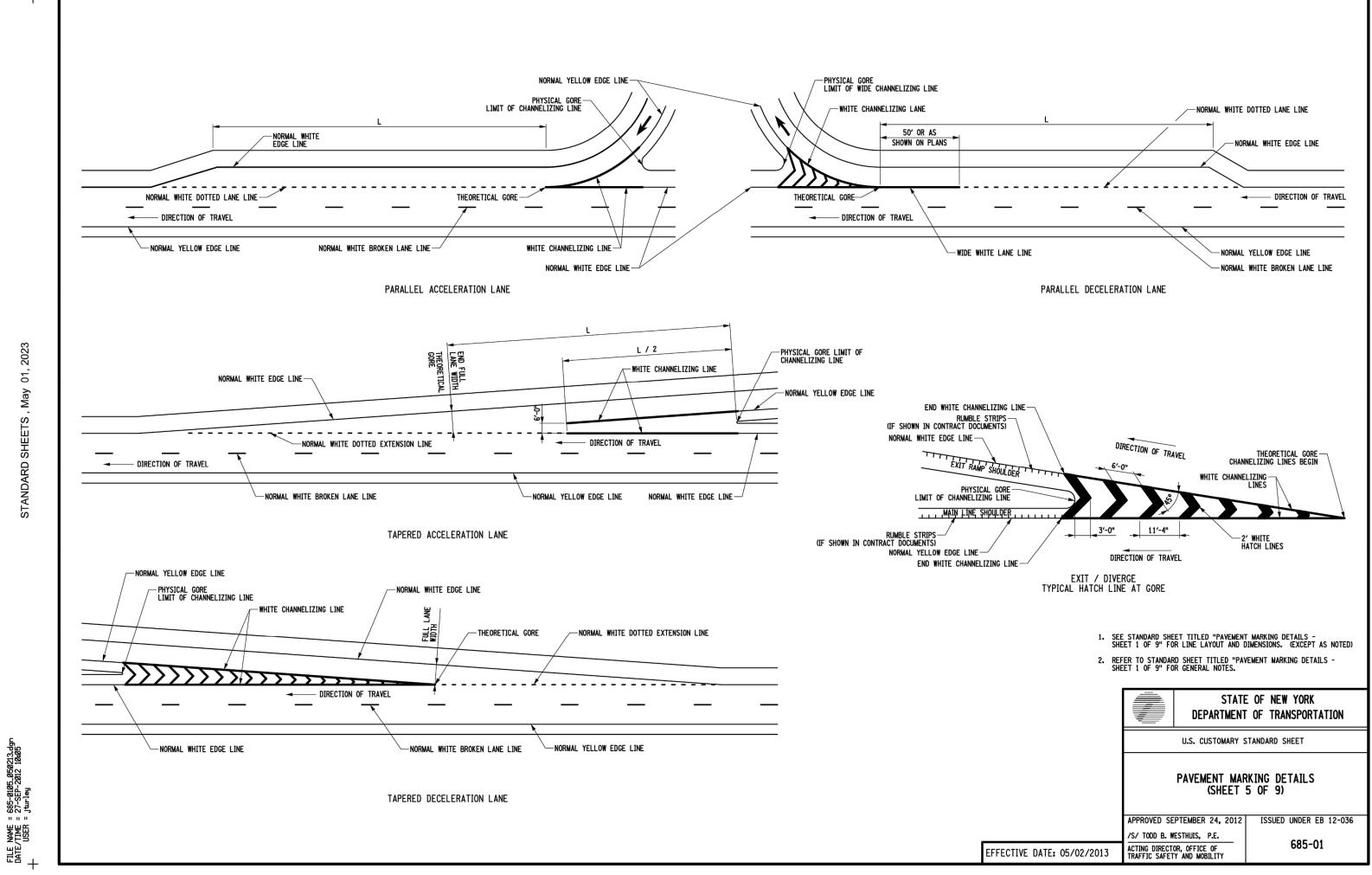


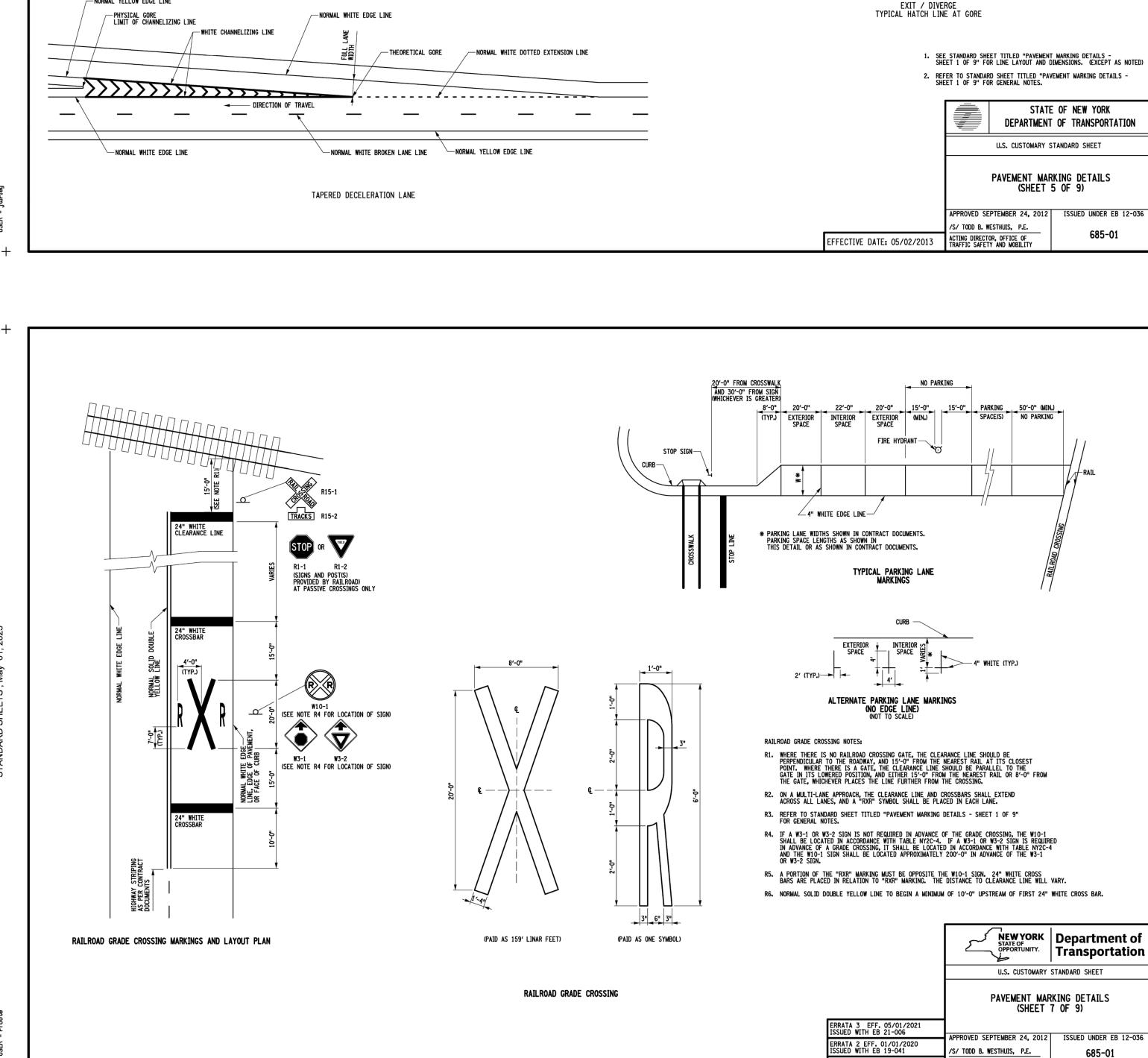
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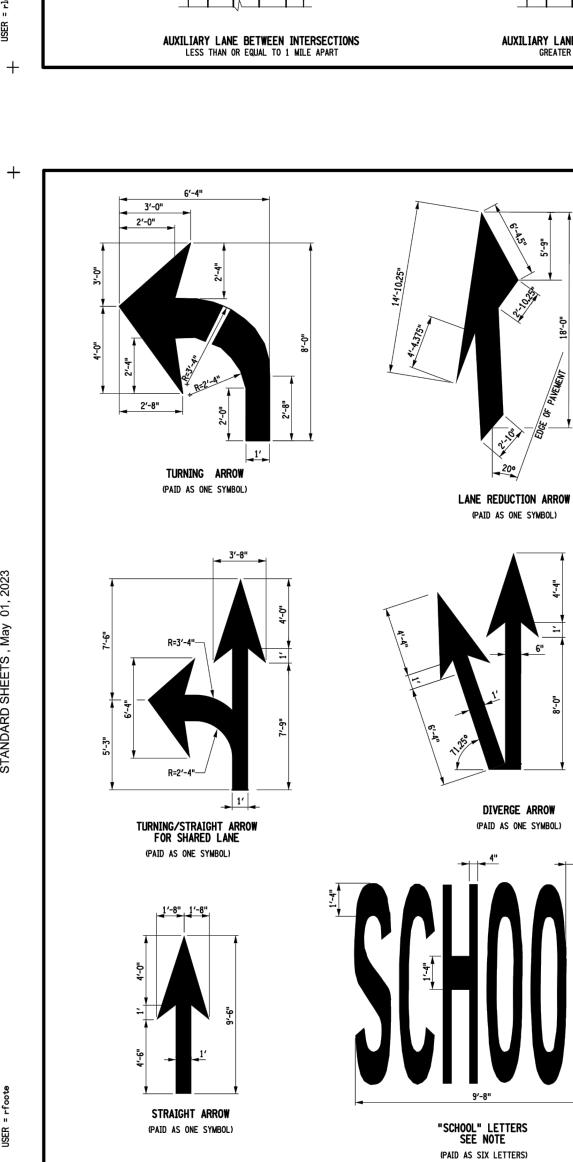








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ERRATA 1 EFF. 01/01/2018 ISSUED WITH EB 17-041

ACTING DIRECTOR, OFFICE OF TRAFFIC SAFETY AND MOBILITY

